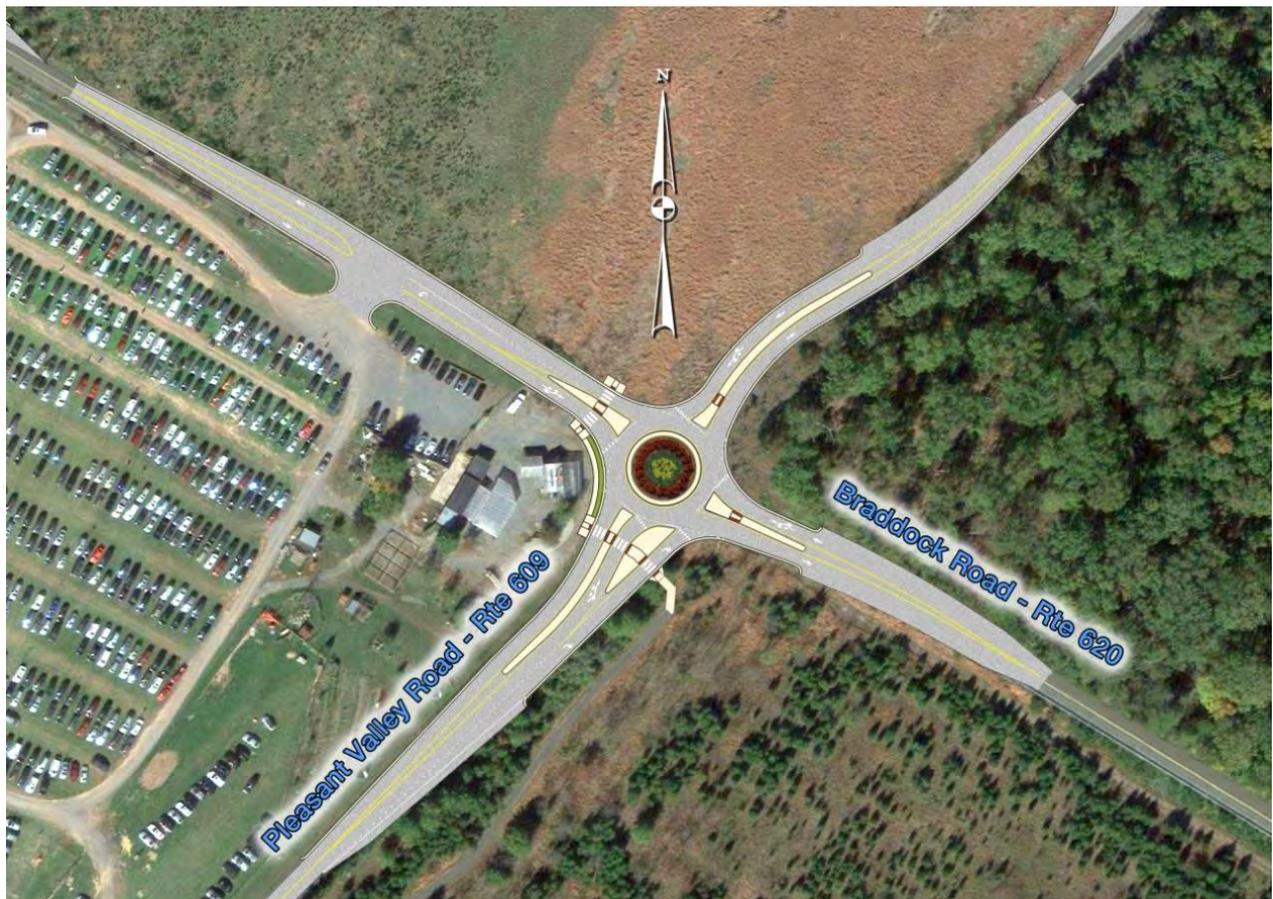


DESIGN-BUILD PROJECT

Braddock Road (Route 620) and Pleasant Valley Road (Route 609) Intersection Improvement in Fairfax County, VA

State Project No.: (NFO) 0620-029-017, P101, R201, C501
Federal Project No.: STP-5A01 (382)
Contract ID No.: C00103318DB71



Submitted By:



**FORT MYER
CONSTRUCTION**

In Association With:



LETTER OF SUBMITTAL CHECKLIST AND CONTENTS

ATTACHMENT 4.0.1.1

ATTACHMENT 4.0.1.1
Braddock Road (Route 620) and Pleasant Valley Road (Route 609)
Intersection Improvement in Fairfax County, Virginia
LETTER OF SUBMITTAL CHECKLIST AND CONTENTS

Offerors shall furnish a copy of this letter of Submittal Checklist, with the page references added, with the Letter of Submittal.

Letter of Submittal Component	Form (if any)	RFP Part 1 Cross Reference	Page Reference
Letter of Submittal Checklist and Contents	Attachment 4.0.1.1	Section 4.0.1.1	Tab Section 4.0.1.1
Acknowledgement of RFP, Revisions, and/or Addenda	Attachment 3.4 (Form C-78-RFP)	Sections 3.4; 4.0.1.1	Tab Form C-78-RFP
Letter of Submittal	NA	Sections 4.1	Tab Section 4.1
Letter of Submittal on Offeror's letterhead	NA	Section 4.1.1	Tab Section 4.1
Offeror's full legal name and address	NA	Section 4.1.1	Tab Section 4.1
Authorized representative's original signature	NA	Section 4.1.1	Tab Section 4.1
Declaration of intent	NA	Section 4.1.2	Tab Section 4.1
120 day declaration	NA	Section 4.1.3	Tab Section 4.1
Point of Contact information	NA	Section 4.1.4	Tab Section 4.1
Principal Officer information	NA	Section 4.1.5	Tab Section 4.1
Offeror's Corporate Structure	NA	Section 4.1.6	Tab Section 4.1
Full Legal Name of Lead Contractor and Lead Designer	NA	Section 4.1.7	Tab Section 4.1
Offeror's VDOT prequalification information	NA	Section 4.1.8	Tab Section 4.1
DBE statement confirming Offeror is committed to achieving the required DBE goal	NA	Section 4.1.9	Tab Section 4.1

ATTACHMENT 4.0.1.1
Braddock Road (Route 620) and Pleasant Valley Road (Route 609)
Intersection Improvement in Fairfax County, Virginia
LETTER OF SUBMITTAL CHECKLIST AND CONTENTS

Final Completion Date	NA	Section 4.1.10	Tab Section 4.1
Letter of Submittal Component	Form (if any)	RFP Part 1 Cross Reference	Page Reference
Attachments to the Letter of Submittal	NA	Section 4.2	Tab Section 4.2
Affiliated and/ or Subsidiary Companies	Attachment 4.2.1	Section 4.2.1	Tab Section 4.2.1
Certification Regarding Debarment Forms	Attachment 4.2.2(a) Attachment 4.2.2(b)	Section 4.2.2	Tab Section 4.2.2(a) Tab Section 4.2.2(b)
Offeror's VDOT prequalification information	NA	Section 4.2.3	Tab Section 4.2.3
Evidence of obtaining bonding	NA	Section 4.2.4	Tab Section 4.2.4
Full size copies of DPOR licenses and SCC registrations	NA	Section 4.2.5	Tab Section 4.2.5
SCC registration information - businesses	Attachment 4.2.5	Section 4.2.5.1	Tab Section 4.2.5.1
DPOR registration information - businesses	Attachment 4.2.5	Section 4.2.5.2	Tab Section 4.2.5.2
Lead Contractor Work History Form	Attachment 4.2.6(a)	Section 4.2.6	Tab Section 4.2.6(a)
Lead Designer Work History Form	Attachment 4.2.6(b)	Section 4.2.6	Tab Section 4.2.6(b)
Conceptual Roadway Plans	NA	Section 4.2.7	Tab Section 4.2.7
Fully Compliant Technical Proposal	NA	Section 4.2.8	Tab Section 4.2.8
CD-ROM	NA	NA	

ACKNOWLEDGEMENT OF RFP REVISIONS AND/OR ADDENDA

ATTACHMENT 3.4 (Form C-78-RFP)

ATTACHMENT 3.4**COMMONWEALTH OF VIRGINIA
DEPARTMENT OF TRANSPORTATION**

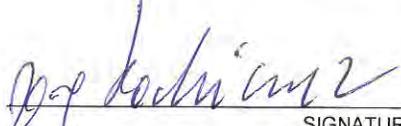
RFP NO. C00103318DB71
 PROJECT NO.: (NFO) 0620-029-017,P101, R201, C501

ACKNOWLEDGEMENT OF RFP, REVISION AND/OR ADDENDA

Acknowledgement shall be made of receipt of the Request for Proposals (RFP) and/or any and all revisions and/or addenda pertaining to the above designated project which are issued by the Department prior to the Letter of Submittal submission date shown herein. Failure to include this acknowledgement in the Letter of Submittal may result in the rejection of your proposal.

By signing this Attachment 3.4, the Offeror acknowledges receipt of the RFP and/or following revisions and/or addenda to the RFP for the above designated project which were issued under cover letter(s) of the date(s) shown hereon:

1. Cover letter of February 25, 2014- RFP
(Date)
2. Cover letter of Addendum #1- April 8, 2014
(Date)
3. Cover letter of Addendum #2- RFP
Information Package- April 15, 2014
(Date)
4. Cover letter of Addendum #3- April 17, 2014
(Date)



 SIGNATURE
 Jose Rodriguez, President
 Fort Myer Construction Corporation

April 24, 2014

 DATE

LETTER OF SUBMITTAL

SECTION 4.1

LETTER OF SUBMITTAL ON OFFEROR'S LETTERHEAD

Sections 4.1.1



FORT MYER CONSTRUCTION CORPORATION

2237 33rd Street, NE • Washington, DC • 20018 | p: 202.636.9535 | f: 202.526.8572

April 24, 2014

Brenda L. Williams
Virginia Department of Transportation (VDOT)
Central Office Mail Center
Loading Dock Entrance
1401 East Broad Street
Richmond, VA 23219
P: 804.786.6929
F: 804.786.7221

Re: Design-Build Project Proposal
Braddock Road (Route 620) and Pleasant Valley Road (Route 609) Intersection Improvement
From: .168 miles west of Pleasant Valley Road
To: .076 miles east of Pleasant Valley Road
Fairfax County, VA
Project No. (NFO) 0620-029-017, P101, R201, C501 | Contract ID # C00103318DB71

Dear Ms. Williams:

Fort Myer Construction Corporation (FMCC) is pleased to present our proposal for the Braddock Road and Pleasant Valley Road Intersection Improvement Design-Build project. As requested by the Department's RFP, our submission includes:

- One (1) original paper version of our Letter of Submittal and Attachments
- One (1) CD-ROM containing the entire original in a single PDF file
- One (1) original paper copy of the Price Proposal, submitted in a separate package
- One (1) CD-ROM containing the entire Price Proposal in a single PDF file, also submitted in the separate package

Fort Myer has thoroughly reviewed the Department's RFP. Following are responses to information and/or attachments requested in section 4.1.

4.1.2 Offerors' Intent: If selected, FMCC intends to enter into a contract with VDOT for the project in accordance with the terms of the RFP.

4.1.3 Price Proposal Timeframe: The price proposal will remain in full force and effect for one hundred twenty (120) days after the date this proposal is submitted to VDOT (April 24, 2014).

4.1.4 Point of Contact: Manuel Fernandes – Vice President. Address: 2237 33rd Street, NE, Washington, DC 20018. He can be reached by phone at 202.636.9535, x2805, by fax at 202.526.8572, and by email at mfernandes@fortmyer.com.

4.1.5 Principal Officer: Jose Rodriguez – President. Address: 2237 33rd Street, NE, Washington, DC 20018. He can be reached by phone at 202.636.9535, by fax at 202.526.8572, and by email at jrodriguez@fortmyer.com.

4.1.6 Corporate Structure: FMCC will be the design-build contracting entity for the Braddock Road and Pleasant Valley Road Intersection Improvement Design-Build project. Fort Myer is a corporation titled in the District of Columbia and will be the sole major participating firm and responsible party to the design-build contract with the Virginia Department of Transportation (VDOT). FMCC will hold all financial responsibility for the contract (a surety letter is provided in the Appendix).

4.1.7 Lead Contractor and Lead Designer: Fort Myer Construction Corporation is the Lead Contractor for this project, serving as the prime/general contractor responsible for overall construction. Whitney Bailey Cox and Magnani, LLC (WBCM) will be our Lead Designer for the project, meaning the prime design consulting firm responsible for overall design.

4.1.8 VDOT Prequalification Evidence: FMCC is pre-qualified with VDOT (Vendor Number F034 – active) to provide Major Structures, Asphalt Concrete Paving, Portland Cement Concrete Paving, Underground Utilities, and Bridge Repairs. The standard VDOT prequalification certificate is presented as Attachment 4.2.3 in the Appendix.

4.1.9 DBE Requirements: FMCC is committed to achieving a twenty percent (20%) DBE participation goal for the entire value of the contract.

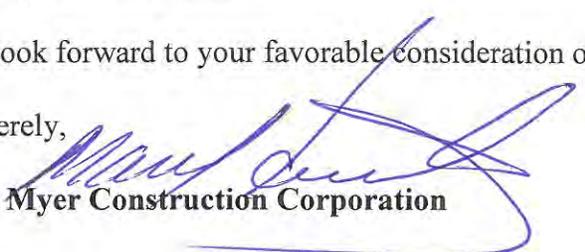
4.1.10 Interim Milestone and Final Completion Dates: As set forth in Part 1, Section 2.4.1 of the RFP, the Final Completion date is 5/4/2016.

This proposal is signed in ink by an authorized representative of Fort Myer Construction Corporation.

The Fort Myer team is most interested in serving the Virginia Department of Transportation and the various project stakeholders. Accordingly, we present to you a design-build team equipped with the experience, knowledge, and resources to successfully deliver the Braddock Road and Pleasant Valley Road Intersection Improvement project, in partnership with VDOT and with comprehensive care for the impacts of the work.

We look forward to your favorable consideration of our proposal.

Sincerely,


Fort Myer Construction Corporation

Manuel Fernandes, FMCC Vice President
mfernandes@fortmyer.com

ATTACHMENTS TO THE LETTER OF SUBMITTAL

SECTION 4.2

AFFILIATED AND/OR SUBSIDIARY COMPANIES

ATTACHMENT 4.2.1

CERTIFICATION REGARDING DEBARMENT FORMS

ATTACHMENT 4.2.2(a)

ATTACHMENT 4.2.2(a)
CERTIFICATION REGARDING DEBARMENT
PRIMARY COVERED TRANSACTIONS

Project No.: (NFO) 0620-029-017, P101, R201, C501

1) The prospective primary participant certifies to the best of its knowledge and belief, that it and its principals:

a) Are not presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from covered transactions by any Federal department or agency.

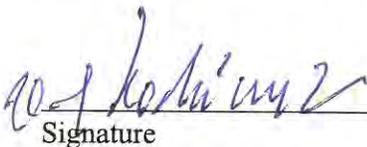
b) Have not within a three-year period preceding this proposal been convicted of or had a civil judgment rendered against them for commission of fraud or a criminal offense in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; and have not been convicted of any violations of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification, or destruction of records, making false statements, or receiving stolen property;

c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated in paragraph 1) b) of this certification; and

d) Have not within a three-year period preceding this application/proposal had one or more public transactions (Federal, State or local) terminated for cause or default.

2) Where the prospective primary participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.



Signature

April 24, 2014

Date

Jose Rodriguez, President

Title

Fort Myer Construction Corporation

Name of Firm

CERTIFICATION REGARDING DEBARMENT FORMS

ATTACHMENT 4.2.2(b)

ATTACHMENT 4.2.2(b) CERTIFICATION
REGARDING DEBARMENT LOWER TIER
COVERED TRANSACTIONS

Project No.: (NFO) 0620-029-017, P101, R201, C501

1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.

2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.

<u>Leon Kriebel</u>	<u>4/1/14</u>	<u>President</u>
Signature (Leon Kriebel, P.E.)	Date	Title

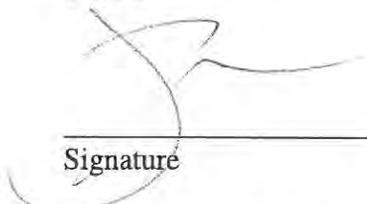
Whitney Bailey Cox & Magnani, LLC
Name of Firm

CERTIFICATION REGARDING DEBARMENT
LOWER TIER COVERED TRANSACTIONS
(To be completed by a Sub-consultant)

Project: On-Call Traffic Engineering NRO and ERO

- 1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.
- 2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the offeror for contracts to be let by the Commonwealth Transportation Board.

 _____ Signature	<u>04/4/2014</u> _____ Date	<u>Principal</u> _____ Title
<u>Sabra, Wang & Associates, Inc.</u> _____ Name of Firm		

ATTACHMENT 4.2.2(b)
CERTIFICATION REGARDING DEBARMENT
LOWER TIER COVERED TRANSACTIONS

Project No.: (NFO) 0620-029-017, P101, R201, C501

- 1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.

- 2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.



April 11, 2014

Executive Vice President

Signature

Date

Title

Wetland Studies and Solutions, Inc.

Name of Firm

ATTACHMENT 4.2.2(b) CERTIFICATION
REGARDING DEBARMENT LOWER TIER
COVERED TRANSACTIONS

Project No.: (NFO) 0620-029-017, P101, R201, C501

1) The prospective lower tier participant certifies, by submission of this proposal, that neither it nor its principals is presently debarred, suspended, proposed for debarment, declared ineligible, or voluntarily excluded from participation in this transaction by any Federal department or agency.

2) Where the prospective lower tier participant is unable to certify to any of the statements in this certification, such prospective participant shall attach an explanation to this proposal.

The undersigned makes the foregoing statements to be filed with the proposal submitted on behalf of the Offeror for contracts to be let by the Commonwealth Transportation Board.



5/2/14

President

Signature

Date

Title

GeoConcepts Engineering, Inc.

Name of Firm

OFFEROR'S VDOT PREQUALIFICATION INFORMATION

SECTION 4.2.3

EVIDENCE OF OBTAINING BONDING

SECTION 4.2.4



April 24, 2014

Mr. Bryan W. Stevenson, P.E.
Virginia Department of Transportation
1401 East Broad Street
Annex Building, 8th Floor
Richmond, VA 23219

RE: Braddock Rd (Route 620) and Pleasant Valley Rd (Route 609)
Project No. (NFO)0620-029-017,P101,R201, C501
Contract ID # C00103318DB71

Dear Mr. Stevenson:

As surety for, Fort Myer Construction Corporation, Western Surety Company with AM Best Financial Strength Rating (A) and Financial Size Category (XII) is capable of obtaining 100% Performance Bond and 100% Labor and Materials Payment Bond in the amount of the anticipated cost of construction, and said bonds will cover the Project and any warranty periods as provided for in the Contract Documents on behalf of the Contractor, in the event that such firm be the successful bidder and enter into a contract for this project.

Best regards,

A handwritten signature in blue ink that reads "Don K Kawamoto". The signature is written in a cursive, flowing style.

Western Surety Company
Don K Kawamoto, Attorney-in-fact

Western Surety Company

POWER OF ATTORNEY APPOINTING INDIVIDUAL ATTORNEY-IN-FACT

Know All Men By These Presents, That WESTERN SURETY COMPANY, a South Dakota corporation, is a duly organized and existing corporation having its principal office in the City of Sioux Falls, and State of South Dakota, and that it does by virtue of the signature and seal herein affixed hereby make, constitute and appoint

Joseph G Delaney, Karen M Earp, Don K Kawamoto, Individually

of Potomac, MD, its true and lawful Attorney(s)-in-Fact with full power and authority hereby conferred to sign, seal and execute for and on its behalf bonds, undertakings and other obligatory instruments of similar nature

- In Unlimited Amounts -

and to bind it thereby as fully and to the same extent as if such instruments were signed by a duly authorized officer of the corporation and all the acts of said Attorney, pursuant to the authority hereby given, are hereby ratified and confirmed.

This Power of Attorney is made and executed pursuant to and by authority of the By-Law printed on the reverse hereof, duly adopted, as indicated, by the shareholders of the corporation.

In Witness Whereof, WESTERN SURETY COMPANY has caused these presents to be signed by its Vice President and its corporate seal to be hereto affixed on this 14th day of October, 2013.

WESTERN SURETY COMPANY

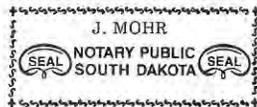


Paul T. Bruflat
Paul T. Bruflat, Vice President

State of South Dakota }
County of Minnehaha } ss

On this 14th day of October, 2013, before me personally came Paul T. Bruflat, to me known, who, being by me duly sworn, did depose and say: that he resides in the City of Sioux Falls, State of South Dakota; that he is the Vice President of WESTERN SURETY COMPANY described in and which executed the above instrument; that he knows the seal of said corporation; that the seal affixed to the said instrument is such corporate seal; that it was so affixed pursuant to authority given by the Board of Directors of said corporation and that he signed his name thereto pursuant to like authority, and acknowledges same to be the act and deed of said corporation.

My commission expires
June 23, 2015



J. Mohr
J. Mohr, Notary Public

CERTIFICATE

I, L. Nelson, Assistant Secretary of WESTERN SURETY COMPANY do hereby certify that the Power of Attorney hereinabove set forth is still in force, and further certify that the By-Law of the corporation printed on the reverse hereof is still in force. In testimony whereof I have hereunto subscribed my name and affixed the seal of the said corporation this 24th day of April, 2014.



WESTERN SURETY COMPANY

L. Nelson
L. Nelson, Assistant Secretary

Authorizing By-Law

ADOPTED BY THE SHAREHOLDERS OF WESTERN SURETY COMPANY

This Power of Attorney is made and executed pursuant to and by authority of the following By-Law duly adopted by the shareholders of the Company.

Section 7. All bonds, policies, undertakings, Powers of Attorney, or other obligations of the corporation shall be executed in the corporate name of the Company by the President, Secretary, and Assistant Secretary, Treasurer, or any Vice President, or by such other officers as the Board of Directors may authorize. The President, any Vice President, Secretary, any Assistant Secretary, or the Treasurer may appoint Attorneys in Fact or agents who shall have authority to issue bonds, policies, or undertakings in the name of the Company. The corporate seal is not necessary for the validity of any bonds, policies, undertakings, Powers of Attorney or other obligations of the corporation. The signature of any such officer and the corporate seal may be printed by facsimile.

FULL SIZE COPIES OF DPOR LICENSES AND SCC REGISTRATIONS

SECTION 4.2.5

SCC REGISTRATION INFORMATION - BUSINESSES

ATTACHMENT 4.2.5.1

Commonwealth of Virginia



State Corporation Commission

CERTIFICATE OF GOOD STANDING

I Certify the Following from the Records of the Commission:

That FORT MYER CONSTRUCTION CORPORATION is duly incorporated under the law of the Commonwealth of Virginia;

That the date of its incorporation is February 11, 1974;

That the period of its duration is perpetual; and

That the corporation is in existence and in good standing in the Commonwealth of Virginia as of the date set forth below.

Nothing more is hereby certified.



*Signed and Sealed at Richmond on this Date:
June 10, 2013*

Joel H. Peck
Joel H. Peck, Clerk of the Commission

ARTICLES OF INCORPORATION

The undersigned, for the purpose of organizing a corporation pursuant to the laws of the state of Virginia, does hereby adopt the following articles of incorporation:

Article I The name of this corporation shall be the FORT MYER CONSTRUCTION CORPORATION.

Article II The general nature of its business shall be to carry on and conduct a contracting business, including the designing, constructing, enlarging, repairing, remodeling or otherwise engaging in any work upon roads, sidewalks, highways, bridges, or buildings including office buildings, manufacturing plants, warehouses and private homes; and to engage in concrete, stone, brick, cement, masonry and earth construction, and to execute contracts or to receive assignments of contracts therefor, or relating thereto.

Article III The aggregate number of shares of stock which the corporation has authority to issue is Thirty Thousand (30,000), all of which are one dollar (\$1.00) par value and all of which are designated as Common Stock.

Article IV At each election of directors every stockholder entitled to vote at such election shall have the right to cumulate his votes, as provided by section 13.1 - 32 of the Code of Virginia.

Article V The address of the corporation's initial registered office is 1914 North 15th Street, Arlington, Virginia 22201, in Arlington County, Virginia, and the name of its initial registered agent at such address is Jose Rodrigues, who is a resident of Virginia and a director of the corporation.

Article VI The number of directors constituting the initial board of directors is three (3). The names and addresses of the persons who are to serve as the initial directors are as follows:

Jose Rodrigues of 1405 N. Scott Street,
Arlington, Virginia

Francisco Rodrigues Neto of 1912 N. 13th Street,
Arlington, Virginia

Lewis F. Shrensky of 11300 Palisades Court,
Kensington, Maryland

In Witness Whereof, I have hereunto set my hand and seal
this 7 day of February, 1974.

SEAL

Lewis F. Shrensky

Commonwealth OF Virginia



State Corporation Commission

CERTIFICATE OF FACT

I Certify the Following from the Records of the Commission:

That Whitney, Bailey, Cox & Magnani, LLC, a limited liability company organized under the law of Maryland, obtained a certificate of registration to transact business in Virginia from the Commission on March 13, 2001; and

That it is registered to transact business in the Commonwealth of Virginia as of the date set forth below.

Nothing more is hereby certified.

*Signed and Sealed at Richmond on this Date:
April 8, 2014*



Joel H. Peck

Joel H. Peck, Clerk of the Commission

Commonwealth OF Virginia



State Corporation Commission

CERTIFICATE OF GOOD STANDING

I Certify the Following from the Records of the Commission:

That SABRA, WANG & ASSOCIATES, INC., a corporation incorporated under the law of Maryland, is authorized to transact business in the Commonwealth of Virginia;

That it obtained a certificate of authority to transact business in Virginia from the Commission on June 30, 1998; and

That the corporation is in good standing in the Commonwealth of Virginia as of the date set forth below.

Nothing more is hereby certified.



Signed and Sealed at Richmond on this Date:

June 6, 2012

Joel H. Peck

Joel H. Peck, Clerk of the Commission

Commonwealth OF Virginia



State Corporation Commission

CERTIFICATE OF GOOD STANDING

I Certify the Following from the Records of the Commission:

That Edwards Utility Mapping Corp. is duly incorporated under the law of the Commonwealth of Virginia;

That the date of its incorporation is April 3, 2009;

That the period of its duration is perpetual; and

That the corporation is in existence and in good standing in the Commonwealth of Virginia as of the date set forth below.

Nothing more is hereby certified.



*Signed and Sealed at Richmond on this Date:
March 24, 2014*

Joel H. Peck

Joel H. Peck, Clerk of the Commission

Commonwealth of Virginia

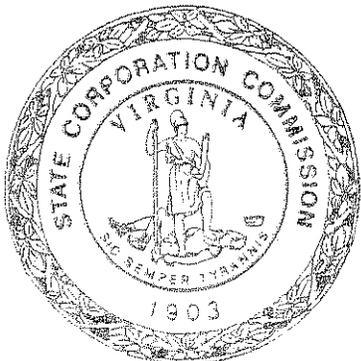


STATE CORPORATION COMMISSION

Richmond, October 18, 1991

This is to Certify that the certificate of incorporation of
WETLAND STUDIES AND SOLUTIONS, INC.

*was this day issued and admitted to record in this office
and that the said corporation is authorized to transact its
business subject to all the laws of the State applicable to the
corporation and its business. Effective date: October 18, 1991*



State Corporation Commission

William J. Bridge

Clerk of the Commission

Commonwealth OF Virginia



State Corporation Commission

CERTIFICATE OF GOOD STANDING

I Certify the Following from the Records of the Commission:

That GeoConcepts Engineering, Inc. is duly incorporated under the law of the Commonwealth of Virginia;

That the date of its incorporation is February 25, 1999;

That the period of its duration is perpetual; and

That the corporation is in existence and in good standing in the Commonwealth of Virginia as of the date set forth below.

Nothing more is hereby certified.



*Signed and Sealed at Richmond on this Date:
July 29, 2013*

Joel H. Peck

Joel H. Peck, Clerk of the Commission

DPOR REGISTRATION INFORMATION - BUSINESSES

ATTACHMENT 4.2.5.2

DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

EXPIRES ON
12-31-2015

NUMBER
0407001741

BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY REGISTRATION

PROFESSIONS: ENG

WHITNEY, BAILEY, COX AND MAGNANI
849 FAIRMOUNT AVENUE
BALTIMORE, MD 21286



Gordon N. Dixon
Gordon N. Dixon, Director

ALTERATION OF THIS DOCUMENT, USE AFTER EXPIRATION, OR USE BY PERSONS OR FIRMS OTHER THAN THOSE NAMED MAY RESULT IN CRIMINAL PROSECUTION UNDER THE CODE OF VIRGINIA.

(SEE REVERSE SIDE FOR NAME AND/OR ADDRESS CHANGE)

(POCKET CARD)

COMMONWEALTH OF VIRGINIA

BOARD FOR APESCIDLA
BUSINESS ENTITY REGISTRATION
NUMBER: 0407001741 EXPIRES: 12-31-2015
PROFESSIONS: ENG
WHITNEY, BAILEY, COX AND MAGNANI
849 FAIRMOUNT AVENUE
BALTIMORE, MD 21286



(FOLD)

(DETACH HERE)

DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
9960 Mayland Dr., Suite 400, Richmond, VA 23233

ALTERATION OF THIS DOCUMENT, USE AFTER EXPIRATION, OR USE BY PERSONS OR FIRMS OTHER THAN THOSE NAMED MAY RESULT IN CRIMINAL PROSECUTION UNDER THE CODE OF VIRGINIA.

10010 (7/11) 107028-3

**DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA**

**EXPIRES ON
12-31-2015**

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

**NUMBER
0407005636**

**BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY REGISTRATION**

PROFESSIONS: ENG

**SABRA, WANG & ASSOCIATES, INC
101 W BROAD ST
SUITE 301
FALLS CHURCH, VA 22046**



Gordon N. Dixon
Gordon N. Dixon, Director

ALTERATION OF THIS DOCUMENT, USE AFTER EXPIRATION, OR USE BY PERSONS OR FIRMS OTHER THAN THOSE NAMED MAY RESULT IN CRIMINAL PROSECUTION UNDER THE CODE OF VIRGINIA.

(SEE REVERSE SIDE FOR NAME AND/OR ADDRESS CHANGE)

(POCKET CARD)

**COMMONWEALTH OF VIRGINIA
BOARD FOR APPLSCIDLA
BUSINESS ENTITY REGISTRATION
NUMBER: 0407005636 EXPIRES: 12-31-2015
PROFESSIONS: ENG
SABRA, WANG & ASSOCIATES, INC
101 W BROAD ST
SUITE 301
FALLS CHURCH, VA 22046**



(FOLD)

(DETACH HERE)

**DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
9960 Mayland Dr., Suite 400, Richmond, VA 23233**

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DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA

EXPIRES ON
12-31-2015

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

NUMBER
0407005544

BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY REGISTRATION

PROFESSIONS: LS

EDWARDS UTILITY MAPPING CORP
11 MARSH RUN ROAD
FREDERICKSBURG, VA 22406



Gordon N. Dixon
Gordon N. Dixon, Director

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COMMONWEALTH OF VIRGINIA

BOARD FOR APELSCIDLA
BUSINESS ENTITY REGISTRATION
NUMBER: 0407005544 EXPIRES: 12-31-2015
PROFESSIONS: LS
EDWARDS UTILITY MAPPING CORP
11 MARSH RUN ROAD
FREDERICKSBURG, VA 22406



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DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
9960 Mayland Dr., Suite 400, Richmond, VA 23233

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COMMONWEALTH OF VIRGINIA

EXPIRES ON
12-31-2015

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

NUMBER
0407003355

BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY REGISTRATION

PROFESSIONS: LS, LA, ENG

WETLAND STUDIES AND SOLUTIONS INC
5300 WELLINGTON BRANCH DR
STE 100
GAINESVILLE, VA 20155



Gordon N. Dixon
Gordon N. Dixon, Director

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COMMONWEALTH OF VIRGINIA
BOARD FOR APPEALS
BUSINESS ENTITY REGISTRATION
NUMBER: 0407003355 EXPIRES: 12-31-2015
PROFESSIONS: LS, LA, ENG
WETLAND STUDIES AND SOLUTIONS INC
5300 WELLINGTON BRANCH DR
STE 100
GAINESVILLE, VA 20155



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DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
9960 Mayland Dr., Suite 400, Richmond, VA 23233

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DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
COMMONWEALTH OF VIRGINIA

EXPIRES ON

12-31-2015

9960 Mayland Dr., Suite 400, Richmond, VA 23233
Telephone: (804) 367-8500

NUMBER

0407004404

BOARD FOR ARCHITECTS, PROFESSIONAL ENGINEERS, LAND SURVEYORS, CERTIFIED INTERIOR DESIGNERS
AND LANDSCAPE ARCHITECTS
BUSINESS ENTITY REGISTRATION

PROFESSIONS: ENG

GEOCONCEPTS ENGINEERING INC
19955 HIGHLAND VISTA DRIVE
SUITE 170
ASHBURN, VA 20147



Gordon N. Dixon
Gordon N. Dixon, Director

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(POCKET CARD)

COMMONWEALTH OF VIRGINIA

BOARD FOR APESCIDLA
BUSINESS ENTITY REGISTRATION
NUMBER: 0407004404 EXPIRES: 12-31-2015
PROFESSIONS: ENG
GEOCONCEPTS ENGINEERING INC
19955 HIGHLAND VISTA DRIVE
SUITE 170
ASHBURN, VA 20147



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DEPARTMENT OF PROFESSIONAL AND OCCUPATIONAL REGULATION
9960 Mayland Dr., Suite 400, Richmond, VA 23233

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LEAD CONTRACTOR WORK HISTORY FORM

ATTACHMENT 4.2.6(a)

ATTACHMENT 4.2.6(a)

LEAD CONTRACTOR - WORK HISTORY FORM

(LIMIT 1 PAGE PER PROJECT)

a. Project Name & Location	b. Name of the prime design consulting firm responsible for the overall project design.	c. Contact information of the Client or Owner and their Project Manager who can verify Firm's responsibilities.	d. Contract Completion Date (Original)	e. Contract Completion Date (Actual or Estimated)	f. Contract Value (in thousands)		g. Dollar Value of Work Performed by the Firm identified as the Lead Contractor for this procurement.(in thousands)
					Original Contract Value	Final or Estimated Contract Value	
Name: I-66 Pavement Rehabilitation Location: Route 50 to Capital Beltway Fairfax County, VA	Name: Volkert & Associates, Inc. served as the lead designer.	Name of Client./ Owner: Virginia Department of Transportation Phone: 703-259-1995 Project Manager: Susan Shaw, P.E. Phone: 703-259-1995 Email: Susan.shaw@VDOT.virginia.gov	November 2012	August 2012	\$38,000	\$46,000	\$46,000

h. Narrative describing the Work Performed by the Firm identified as the Lead Contractor for this procurement. If the Offeror chooses to submit work completed by an affiliated or subsidiary company of the Lead Contractor, identify the full legal name of the affiliate or subsidiary and the role they will have on this Project, so the relevancy of that work can be considered accordingly.

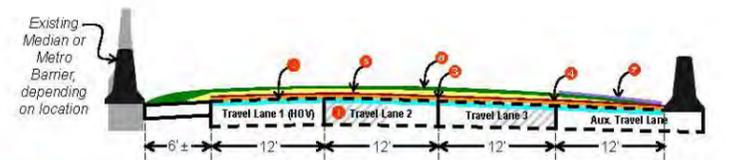
Fort Myer was the lead contractor on this project which consisted of 46,000 square yards of full-depth concrete pavement patching, 140,000 tons of asphalt overlay associated geometric analysis and hydraulic design to maintain drainage and clearances for existing infrastructure, storm drainage, utilities, replacement of existing loop detection with non-intrusive traffic detection units, and coordination on I-66 between Route 50 and I-495, approximately 6.5 miles of interstate roadway.

A critical component of the project required pivotal coordination between, VDOT, Fort Myer, Washington Metro Area Transit Authority (WMATA) and Flour Lane to conduct critical lane closures and perform construction on two of Virginia's highly congested Interstates, I-495 and I-66. As this Design Build project integrated with the Hot Lanes project on I-495, Fort Myer coordinated with Flour-Lane to ensure that traffic restrictions, ramp and lane closures were minimized to reduce impediments to vehicular traffic.

Another component for this project is the integration with VDOT's Intelligent Transportation Systems ("ITS") device upgrade. VDOT's acceptance required testing of counts, speed, classification and alignment of the RTMS units. These units transmit data via the fiber optic cable to the VDOT Traffic Management System. In conjunction with VDOT, Fort Myer was also responsible for implementing the RTMS units into the "Open Roads" Software. "OpenTMS" is the version this project will be migrated into.



Scope of Work



- 1 Patch badly deteriorated concrete pavement with full-depth concrete patches and seal joints
- 2 Patch minor spalling with asphalt
- 3 Seal joints, eradicate pavement markings, remove snow plowable raised pavement markers
- 4 Place 5/8" Thin Hot Mix Asphalt Concrete Overlay stress absorbing membrane interface layer
- 5 2" SMA-12.5 (PG 76-22, polymer modified)
- 6 1-1/2" SMA-9.5 (PG 76-22, polymer modified)
- 7 3/8" High Friction Surface Course to delineate auxiliary travel lane

Fort Myer has performed all aspects of the construction under constraining work hours. This project is a testament to Fort Myer's commitment to safety. Fort Myer Construction is responsible for safely managing the high volumes of traffic through the extensive rehabilitation project.

This project showcases Fort Myer's ability to successfully coordinate with various agencies and existing projects to complete projects within a timely manner with highest quality of work. Fort Myer was able to complete the contract work three months ahead of the original completion date while constructing an additional 10,000 square yards of pavement.

Project Features

- Full and partial depth concrete patching
- Thin Hot Mix Asphalt Overlay (THMACO)
- Safety and hardware updates
- 4"- 4.5" asphalt overlay through the project limits
- Adjusting overhead sign structures
- Drainage modifications
- Significant Public Outreach
- Coordination with corresponding Design-Build projects
- Critical Maintenance of Traffic
- "ITS" Installation & Integration

Lead Contractor: Fort Myer Construction Corporation



Scope and Complexity Similarities

- Urgently needed pavement improvements for a heavily travelled region
- Coordination with corresponding Design-Build projects
- Critical Maintenance of Traffic
- Significant sized project : \$46 million
- VDOT Project
- Combination of state and federal funding

Evidence of Good Performance

I-66 Project has been used as a symbol of excellence and has received awards for paving and its design. The project was also completed ahead of schedule and within the budget.

Lessons Learned

FMCC gained valuable experience working with VDOT on this mega Design Build project. FMCC team is well equipped with staff that can oversee the QA/QC process on large and fast tracked projects like these. FMCC also gained great experience in coordination with several mega projects in the same geographical region.

ATTACHMENT 4.2.6(a)

LEAD CONTRACTOR - WORK HISTORY FORM

(LIMIT 1 PAGE PER PROJECT)

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					Original Contract Value	Final or Estimated Contract Value	
Name: New York Avenue, NE from Florida Avenue/4th, Penn and Neal Streets Location: Washington, DC	Name: T.Y. Lin	Name of Client./ Owner: District of Columbia Department of Transportation Phone: 202.673.6813 Project Manager: Mr. Ali Shakeri, PE Phone: 202.671.4612 Email: Ali.Shakeri@dc.gov	Dec. 2012	Oct. 2013	\$25,000	\$39,000	\$39,000

h. Narrative describing the Work Performed by the Firm identified as the Lead Contractor for this procurement. If the Offeror chooses to submit work completed by an affiliated or subsidiary company of the Lead Contractor, identify the full legal name of the affiliate or subsidiary and the role they will have on this Project, so the relevancy of that work can be considered accordingly.

New York Avenue project includes a value engineering alternative, completed by Fort Myer and T.Y Lin International, consisting of superstructure replacement and substructure rehabilitation of the existing West and East Bound bridges located at New York Avenue, spanning over Amtrak, CSX transportation and WMATA railroads. With concurrent improvement projects occurring on other DC roads, limiting traffic impacts on the already-congested New York Avenue corridor is a top goal of District of Columbia department of transportation. Additional goals of the project include upgrade of existing utility infrastructure, upgrade of superstructure, repair of bridge piers and abutments, improvements to approach roadways, improvements to pedestrian sidewalks and improvements to roadway lighting features.

Specifically this design-build project involves the demolition, removal, lowering and reconstruction of the twin-span New York Avenue bridge superstructure and piers, widening of existing abutments and construction of a new historic sidewalk rail. Construction includes removal of the existing bridge deck, barriers, lighting, girders, beams, bracing, piers and bearings; new beam seats back wall and pier columns; reinforced concrete deck and joints at each abutment; design, installation and removal of temporary structures to support construction and safety protection of construction personnel working over high-voltage wires and rail tracks. Electrical work includes rehabilitating or replacing bridge and roadway lighting; and pavement restoration to New York Avenue to match the elevation change of the bridge abutments.

FMCC engaged T.Y.Lin to provide an innovative design that would address issues such as improved constructability and schedule compressions while achieving cost effectiveness. The proposed value engineering includes retrofitting and re engaging the existing substructure and foundations units to support a new multi-girder superstructure systems. In addition to the structural complexities, other challenges include the maintenance of traffic, coordination with railroad, and the complex geometric layout of the existing structure.

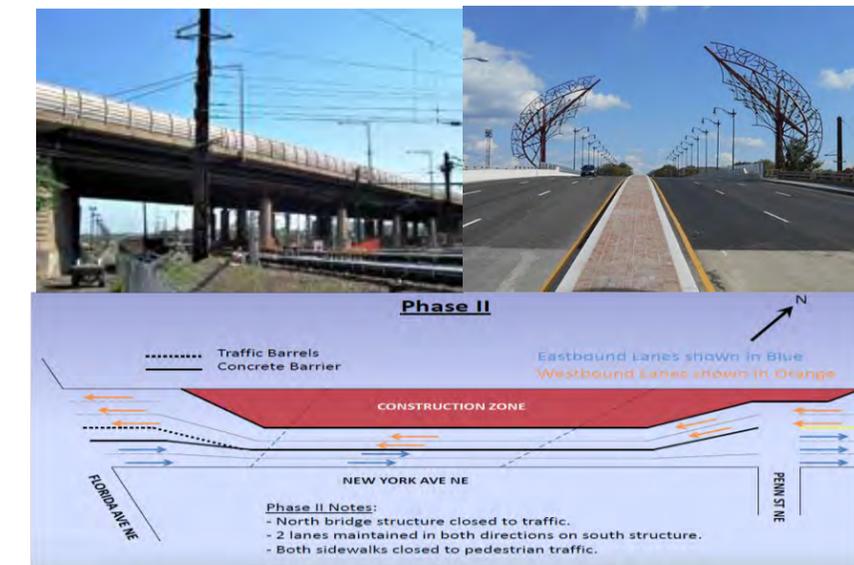
Fort Myer successfully worked with the following partners on this project:



Project Features

- Design-Build Bridge replacement
- Heavily traveled and most congested transportation corridors in District of Columbia
- Concurrent improvement projects in the same corridor
- Public Awareness
- Teamed with T.Y. Lin International and Greenhorne and O'Mara for the design aspect of the project
- Upgrade of existing Amtrak Electrical Infrastructure
- Improvements to roadway lighting fixtures
- Improvements to approach roadways and pedestrian sidewalks

Lead Contractor: Fort Myer Construction Corporation



Scope and Complexity Similarities

- Design-Build project
- Heavily traveled and most congested transportation corridors
- Coordination between concurrent improvement projects in the same region
- Public relations
- Asphalt Paving
- Night time Work

Evidence of Good Performance

Working on such a busy corridor can reveal several challenges when it comes down to maintenance of traffic, but FMCC team handled the MOT with great precision and safety. The project was on time and within the budget.

Lessons Learned

Originally this was a Design-Bid-Build project, which turned into a Design-Build project in a later phase. FMCC team gained valuable experience on this project working with five major stakeholders and satisfying all their demands in a timely manner.

ATTACHMENT 4.2.6(a)

LEAD CONTRACTOR - WORK HISTORY FORM

(LIMIT 1 PAGE PER PROJECT)

a. Project Name & Location	b. Name of the prime design consulting firm responsible for the overall project design.	c. Contact information of the Client or Owner and their Project Manager who can verify Firm's responsibilities.	d. Contract Completion Date (Original)	e. Contract Completion Date (Actual or Estimated)	f. Contract Value (in thousands)		g. Dollar Value of Work Performed by the Firm identified as the Lead Contractor for this procurement.(in thousands)
					Original Contract Value	Final or Estimated Contract Value	
Name: Kenilworth Avenue NE & I-295 Location: Washington, DC	Name: LD CA (Legion Design / Campbell & Associates)	Name of Client./ Owner: District of Columbia Department of Transportation Phone: 202.673.6813 Project Manager: Mr. Ali Shakeri, PE Program Manager, Wards 7&8 Phone: 202-671-4612 Email: Ali.Shakeri@dc.gov	Oct. 2009	Nov. 2010	\$32,983	\$37,141	\$37,141

h. Narrative describing the Work Performed by the Firm identified as the Lead Contractor for this procurement. If the Offeror chooses to submit work completed by an affiliated or subsidiary company of the Lead Contractor, identify the full legal name of the affiliate or subsidiary and the role they will have on this Project, so the relevancy of that work can be considered accordingly.

This project included the reconstruction/construction of five bridges and the roadway reconstruction of Kenilworth Avenue (I-295) in northeast Washington, DC. Major work included three single-span bridges over Watts Branch Creek, one two-span bridge crossing over Nannie Helen Burroughs Avenue NE, and 1/2 mile of roadway reconstruction and rehabilitation of Kenilworth Avenue NE and the adjacent east and west service roads. Successful completion of this project required drilling caissons for bridge piers, driving steel H-piles for the abutment foundations, ground improvement by compaction grouting to stabilize bridge and retaining wall foundations, and constructing 1500 linear feet of Mechanically Stabilized Earth (MSE) walls. Other work included a new 16" water line and storm drain system, street lighting/traffic signal system, and three art structures with special lighting.

As construction had to be performed on one of D.C.'s busiest interstates, Maintenance of Traffic was a critical component. To reduce congestion for nearly 130,000 daily commuters, Fort Myer Construction utilized a movable barrier system to maintain three lanes in any one direction during peak hours. To avoid unnecessary congestion due to easily resolvable incidents, Fort Myer kept a tow truck on site. This proved effective in minimizing delays.

Fort Myer encountered a significant unforeseen site condition in a unmarked 48" sewer line that directly conflicted with the project work. Because Fort Myer possessed the material and supplies necessary to resolve this conflict it was able to complete this project with only minimal delay.

Project Features

- Multiple Bridge replacement
- Full and partial depth concrete repairs
- Asphalt Overlay through the project limits
- Safety and hardware upgrades
- Public Outreach
- Critical maintenance of traffic
- Heavily traveled/highly congested roadway
- MSE walls and Utility relocation

Scope and Complexity Similarities

- Full and partial depth concrete repairs
- Asphalt Overlay through the project limits
- Safety and hardware upgrades
- Public Outreach
- Critical maintenance of traffic
- Heavily traveled/highly congested roadway

Evidence of Good Performance

The project was completed on time and within the budget. There were ZERO accidents on the project and the project was delivered with the highest standard of quality.

Lead Contractor: Fort Myer Construction Corporation

Lessons Learned

FMCC handled this project with a very effective strategy, since the project included five bridges and half mile of roadway reconstruction. There was never a traffic mitigation problem and FMCC hopes to imply the same strategies on projects of similar magnitude and nature.



LEAD DESIGNER WORK HISTORY FORM

ATTACHMENT 4.2.6(b)

ATTACHMENT 4.2.6(b)

LEAD DESIGNER - WORK HISTORY FORM

(LIMIT 1 PAGE PER PROJECT)

a. Project Name & Location	b. Name of the prime/ general contractor responsible for overall construction of the project.	c. Contact information of the Client and their Project Manager who can verify Firm's responsibilities.	d. Construction Contract Completion Date (Original)	e. Construction Contract Completion Date (Actual or Estimated)	f. Contract Value (in thousands)		g. Design Fee for the Work Performed by the Firm identified as the Lead Designer for this procurement.(in thousands)
					Construction Contract Value (Original)	Construction Contract Value (Actual or Estimated)	
Name: US 29 at MD 198 Relocation and Interchange Improvements Location: Montgomery County, MD	Name: Concrete General, Inc.	Name of Client.: MD State Highway Administration Phone: 866-291-9572 Project Manager: Melinda Peters Phone: 410-545-8775 Email: mpeters@sha.state.md.us	2005	2005	\$28,000	\$28,000	\$2,334

h. Narrative describing the Work Performed by the Firm identified as the Lead Designer for this procurement. Include the office location(s) where the design work was performed and whether the firm was the prime designer or a subconsultant.

Whitney Bailey Cox & Magnani, LLC's (WBCM) was the lead design engineer for Concrete General (CGI), the contractor who was the selected in 2002 by the Maryland State Highway Administration (SHA) to design and construct another phase on the US 29 corridor with the SHA's action of improving intersections, interchanges and new alignments for the Montgomery County portion of US 29.

This design-build project included a 7,000 foot relocation of US 29, reconstruction of MD 198, an interchange at the relocated US 29 and MD 198, a roundabout at Dustin Road, and a major expansion of the Burtonsville Park-n-Ride. Project also included retaining walls, MSE slopes, water and sewer line relocations, traffic signals, signing, lighting, pavement markings, and landscaping. WBCM was responsible for the design of the drainage, erosion and sediment controls and stormwater management including 12 stormwater management ponds. In order to begin the construction as quickly as possible, the project was divided into 12 different phases, with construction operations beginning clearing and grubbing soon after the contract was awarded. The project required 675,000 cubic yards of excavation, 640,000 cubic yards of embankment, 13,000 LF of drainage pipe, extensive erosion and sediment control, and five large stormwater management ponds.

The project included 40 guide and 118 regulatory/warning signs, pavement markings, traffic signals, interconnect, and one CMS. Also included was a multi-phase maintenance of traffic plan to perform all construction activities under traffic with minimal disruptions. Temporary traffic signals and interconnect plans were prepared to maintain control during construction. The maintenance of traffic plan also included a detailed plan for maintaining bicycle traffic along the corridor.

During construction, WBCM provided consultation for the contractor's personnel and subcontractors, and SHAs project engineer, participated in Partnering/Construction Progress Meetings, and prepared design studies and plans to resolve construction problems. This method had saved a tremendous amount of time, as work was able to begin on some sections of the roadway while other sections were still being designed. This project was the largest design-build project that SHA has delivered to date at that time.



ATTACHMENT 4.2.6(b)

LEAD DESIGNER - WORK HISTORY FORM

(LIMIT 1 PAGE PER PROJECT)

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					Construction Contract Value (Original)	Construction Contract Value (Actual or Estimated)	
Name: Center Street at Gorsuch Roundabout Location: Carroll County, MD	Name: Concrete General, Inc.	Name of Client.: Carroll County Phone: 410-386-2400 Project Manager: Deborah Butler, P.E. Phone: 410-386-2157 Email: dbutler@ccg.carr.org	2005	2005	\$660	\$660	\$86

h. Narrative describing the Work Performed by the Firm identified as the Lead Designer for this procurement. Include the office location(s) where the design work was performed and whether the firm was the prime designer or a subconsultant.

Whitney Bailey Cox & Magnani, LLCs (WBCM) Baltimore office was the lead designer for this high volume intersection is located just east of the MD 140/MD 97 intersection and immediately adjacent to the Winters Mill High School. The high school was being expanded and the rush hour traffic volumes were increasing and expected to continue to increase for the foreseeable future. The roundabout was the result of traffic studies previously done by WBCM, and was designed to accommodate the increases in traffic demand while slowing traffic and enhancing safety for the school buses and students.

The improvements included construction of a roundabout with flexible pavement and concrete curb & gutter, concrete sidewalks, concrete truck aprons, mountable concrete islands, a new school entrance, grading, drainage, stormwater management, erosion/sediment controls and the installation of w-beam traffic barrier, signing, pavement markings, and landscaping. The project was very successful and appreciated by the community. WBCM participated in a presentation to Winters Mill High School students after the project was completed and they were very enthusiastic about how well the roundabout was working. They wanted to hear all about the design details and some offered sketches with their ideas for various ways of improving roundabout designs.



ATTACHMENT 4.2.6(b)

LEAD DESIGNER - WORK HISTORY FORM

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					Construction Contract Value (Original)	Construction Contract Value (Actual or Estimated)	
Name: MD 30 Relocated Hampstead Bypass Location: Carroll County, MD	Name: Corman Construction, Inc.	Name of Client.: MD State Highway Admin. Phone: 410-545-0300 Project Manager: Jason Ridgway Phone: 410-545-8394 Email: jridgway@sha.state.md.us	8/2009	8/2009	\$40,137	\$43,295	\$2,817

h. Narrative describing the Work Performed by the Firm identified as the Lead Designer for this procurement. Include the office location(s) where the design work was performed and whether the firm was the prime designer or a subconsultant.

Whitney Bailey Cox & Magnani, LLCs (WBCM) Baltimore office was the prime lead design engineer for the design-build of MD 30 Hampstead Bypass around the Town of Hampstead. MD 30 has previously experienced severe congestion in the AM and PM peaks. It was estimated that approximately two-thirds of the traffic on existing MD 30 in Hampstead would divert to the new route enabling the Town of Hampstead to redevelop the downtown area in accordance with its "Main Street Revitalization Plan". WBCM was responsible for the design of the relocation of MD 30 from south of Wolf Hill Drive to north of the Town of Hampstead, a distance of 4.4- miles. The bypass traverses through farm fields crossing over several County roads. WBCM's project design included new roadway, four bridges, three roundabouts, 3,500 LF of noise wall or noise berms, 13 stormwater management facilities, erosion/sediment control, signing, lighting, traffic signals, and water and sewer relocations. The project required 928,000 cubic yards of cut, 760,000 cubic yards of embankment, and extensive erosion/sediment control and landscaping of the entire project.

There are three roundabouts on the project. One is at the southern end of the project which ties the bypass into existing MD 30 and connected a relocated access road (Phillips Drive) into the Wolf Hill subdivision. The existing access to the subdivision which is directly from MD 30 was closed. Another roundabout is located where the bypass intersects MD 482, approximately midway through the project. A third roundabout is located at the northern end of the project and ties the bypass back into existing MD 30.

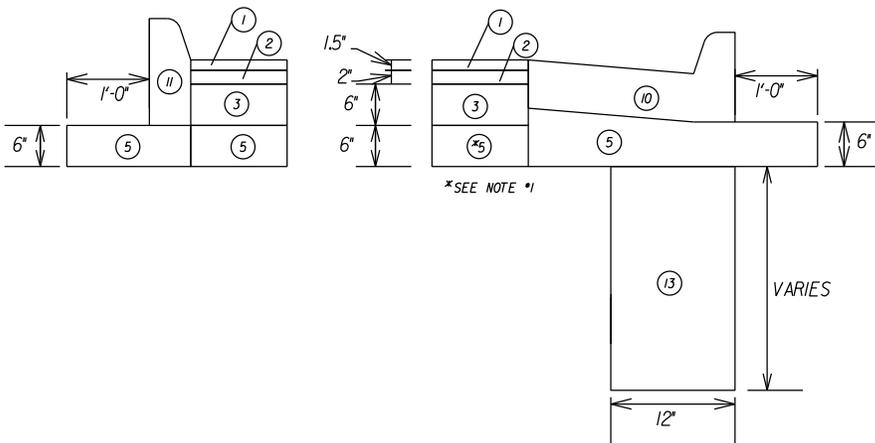
The bypass crossed a bog turtle habitat which is a federally protected species. The project required extreme caution and monitoring in that area to avoid impact to the bog turtle habitat. The project included construction of a bog turtle barrier to prevent the turtles from accessing the roadway. In addition, within the hydrologic influence area of the bog turtle, the project incorporated stormwater management facilities designed to contain a 10,000 gallon accidental spill during a two year storm event.

The project included incorporating the SHA Visual Quality Guidelines into the design of the stormwater management ponds. There were 13 stormwater ponds that were designed with forebays to meet water quality requirements. The bypass crosses some very sensitive environmental areas. There are 10 major streams crossing on the project some which are classified as Use 3 and Use 4 waters. WBCM performed HEC-RAS modeling to size bridge openings.

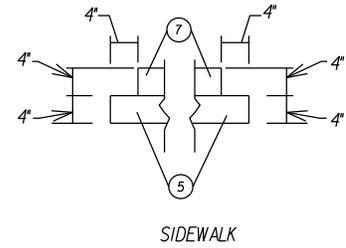


CONCEPTUAL ROADWAY PLANS

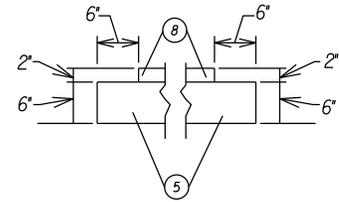
SECTION 4.2.7



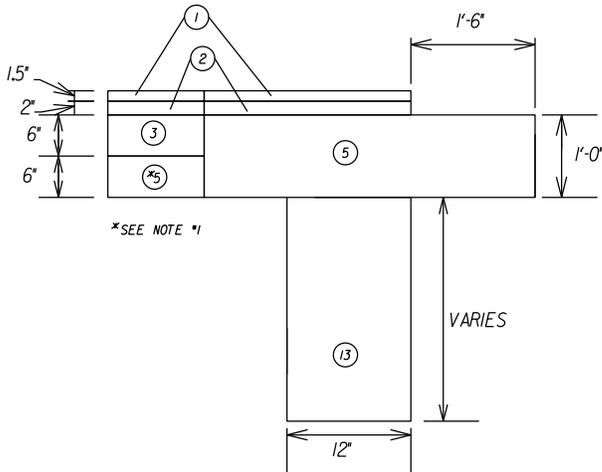
PROPOSED PAVEMENT CURB & GUTTER CLOSED SECTION



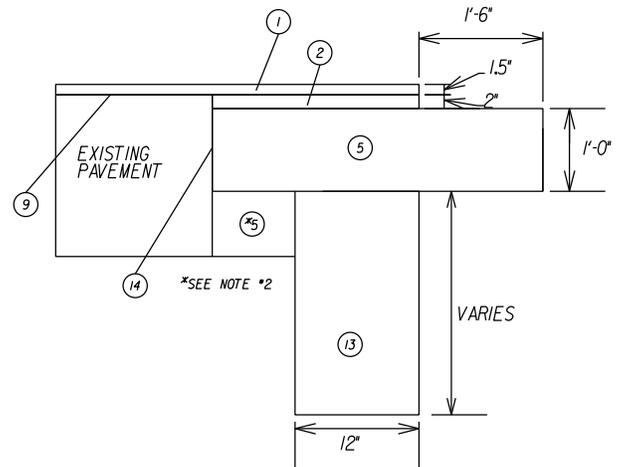
SIDEWALK



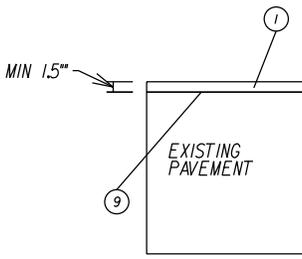
SHARED USED PATH



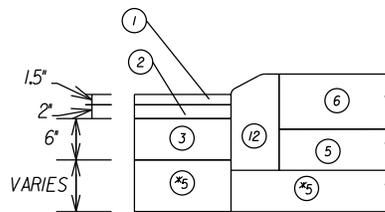
PROPOSED NEW PAVEMENT OR WIDENING PAVEMENT AND FULL DEPTH SHOULDER OPEN SECTION



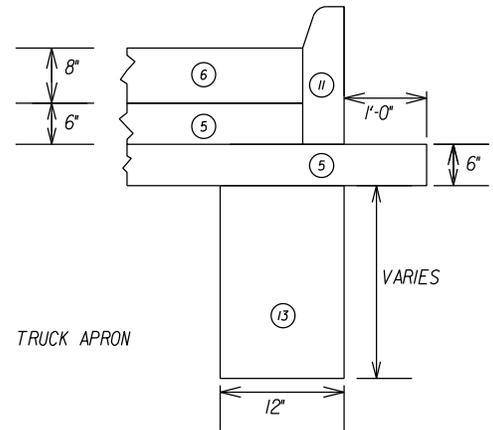
FULL DEPTH SHOULDER OPEN SECTION



OVERLAY SECTION



*SEE NOTE *3



TRUCK APRON

PAVEMENT LEGEND

1. ASPHALT CONCRETE, TYPE SM-9.5D
2. ASPHALT CONCRETE, TYPE IM-19.0D
3. ASPHALT CONCRETE, TYPE BM-25.0A
4. AGGREGATE BASE MATERIAL, TYPE 1, SIZE NO. 21A PUGMILL MIXED WITH 4% HYDRAULIC CEMENT BY WEIGHT
5. AGGREGATE BASE MATERIAL, TYPE 1, SIZE NO. 21B
6. CLASS A3 HYDRAULIC CEMENT CONCRETE PAVEMENT
7. CLASS A3 HYDRAULIC CEMENT CONCRETE
8. ASPHALT CONCRETE, TYPE SM-9.5A
9. EXISTING PAVEMENT SURFACE AFTER MILLING
10. CURB AND GUTTER STANDARD CG-6
11. CURB STANDARD CG-2
12. MODIFIED CG-3
13. UNDER DRAIN UD-4
14. SAW CUT

NOTES

1. WIDENING ON HIGH SIDE OF EXISTING CROSS SLOPE, SHALL HAVE 18 INCHES OF AGGREGATE BASE MATERIAL, TYPE 1, SIZE NO. 21A PUGMILL MIXED WITH 4% HYDRAULIC CEMENT BY WEIGHT. SUBSTITUTED FOR 6 INCHES OF AGGREGATE BASE MATERIAL, TYPE 1, SIZE 21B.
2. AGGREGATE BASE NO. 21B SHALL MEET EXISTING PAVEMENT SECTION DEPTH. TO PROVIDE CONTINUOUS DRAINAGE OF AGGREGATE BASE.
3. ROUNDABOUT AGGREGATE BASE NO. 21B SHALL MEET TRUCK APRON AGGREGATE BASE NO. 21B DEPTH. TO PROVIDE CONTINUOUS DRAINAGE OF AGGREGATE BASE.

FMD FORT MYER CONSTRUCTION
 Since 1972
 IN ASSOCIATION WITH:

WBCM
 ARCHITECTURE ENGINEERING CONSTRUCTION

PAVEMENT SECTIONS

PROJECT MANAGER: Amir Salahsac (703) 259-1957
 SURVEYED BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012
 DESIGN BY: J. Jaramas Group (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	620	0620-029-017, P-101	1A

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

INDEX OF SHEETS

SHEET NO.1	TITLE SHEET
SHEET NO.1A	INDEX AND PROJECT LOCATION MAP
SHEET NO.1B	RIGHT OF WAY DATA SHEET
SHEET NO.1C	SURVEY DATA SHEET
SHEET NO.1D	ALIGNMENT DATA SHEET
SHEET NO.2	TYPICAL SECTIONS
SHEET NO.3	PLAN SHEET - ROUTE 609
SHEET NO.3A	PROFILE SHEET - ROUTE 609
SHEET NO.4	PLAN SHEET - ROUTE 609
SHEET NO.4A	PROFILE SHEET - ROUTE 609
SHEET NO.5	PLAN SHEET - ROUTE 609 & ROUTE 620
SHEET NO.5A	PROFILE SHEET - ROUTE 609 & ROUTE 620
SHEET NO.5B	PLAN SHEET - ROUTE 620
SHEET NO.5C	PROFILE SHEET - ROUTE 620
SHEET NO.5D	PLAN SHEET - ROUTE 620
SHEET NO.5E	PROFILE SHEET - ROUTE 620
SHEET NO.6	PLAN SHEET - ROUTE 609
SHEET NO.6A	PROFILE SHEET - ROUTE 609
SHEET NO.7	SHEET OMITTED
SHEET NO.8(1) - 8(6)	PERMANENT PAVEMENT MARKING AND SIGNING PLANS
SHEET NO.9(1) - 9(3)	ROUNDBOUT LANDSCAPE PLANS

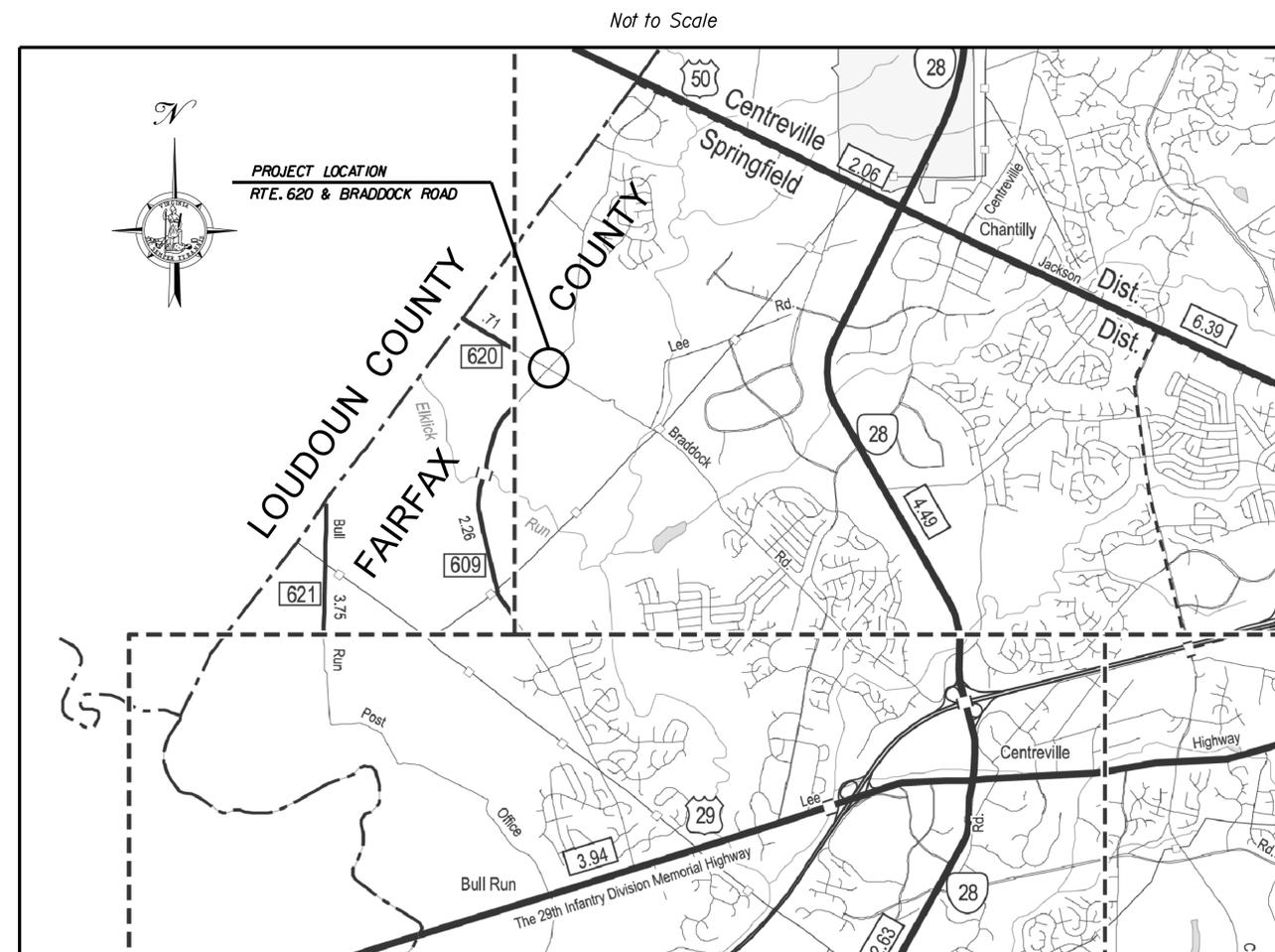
LOCATION MAP

FAIRFAX COUNTY

ROUTE 620
FROM: 0.168 MI. WEST OF RTE. 609 (PLEASANT VALLEY ROAD)
TO: 0.076 MI. EAST OF RTE. 609 (PLEASANT VALLEY ROAD)

RFP CONCEPTUAL PLANS

THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.



PROJECT MANAGER: *Amr. Salahsoor (703) 259-1957*
 SURVEYED BY, DATE: *Leon Treutle, L.S. (VDOT) 12/03/2012*
 DESIGN BY: *TJ Woods, Geop. (804) 200-6500*
 SUBSURFACE UTILITY BY, DATE: *Leon Treutle, L.S. (VDOT) 12/03/2012*

NOTE: ADDITIONAL UTILITY EASEMENTS MAY BE REQUIRED.

ROUTE: *620* PROJECT: *0620-029-017, P-101*
 COUNTY/CITY: *FAIRFAX COUNTY* PPMS NO.: *103318*
 COMPILED BY: *M. FLEMING* DATE: *06/17/2013*
 REVISED BY: DATE:
 REVISED BY: DATE:

REVISED	STATE	STATE		SHEET NO.
	ROUTE	PROJECT		
	VA.	620	0620-029-017, P-101	1B

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

RIGHT OF WAY DATA SHEET

RFP CONCEPTUAL PLANS

THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.

SUBMITTED BY:



IN ASSOCIATION WITH:



PARCEL NO.	LANDOWNER	SHEET NO.	AREA												PROFFERS		
			TOTAL	FEE TAKING	PRESCRIPTIVE R/W	FEE REMAINDER	EASEMENTS						CONSTR.	ENTRANCE RECONSTRUCTION		YES / NO	
							PERMANENT		UTILITY			TEMPORARY					
							SLOPE & DRAINAGE	DRAINAGE	NOVEC	VERIZON	VDOT JOINT-USE	PUBLIC UTILITY					
ACRES	ACRES	ACRES	ACRES	ACRES	ACRES	ACRES	ACRES	ACRES	ACRES	ACRES	ACRES	ACRES					
001	COX-RICHARD FAMILY FARM LLLP	3, 4, 5	13.8084	0.247		13.5614										NO	
002	COX-RICHARD FAMILY FARM LLLP	5, 5B	3.6696	0.271		3.3986										NO	
003	FAIRFAX COUNTY PARK AUTHORITY	5, 5B, 6	39.7630	1.903		37.8600										NO	
004	FAIRFAX COUNTY PARK AUTHORITY	5, 5D, 6	67.2634	0.144		67.1194										NO	
005	FAIRFAX COUNTY PARK AUTHORITY	3, 4, 5, 5D	146.1379	0.043		146.0949										NO	

PROJECT MANAGER: Amr. Salahsac (703) 259-1957
 SURVEYED BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012
 DESIGN BY: T. Trammis Group (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012

Note: To Convert VA State Plane Coordinates NAD 83 Metric Values to VDOT Project Coordinates:
 1. Reduce the Easting 2.5 Million Meters and the South and North Zone Northing by 1 and 2 Million Respectively.
 2. Multiply by the U.S. Survey Foot (3.280833333333333)
 3. Multiply These Values by the Combined Scale and Elevation Factor (1.00006) for this County.
 A Reverse of This Procedure will Transform VDOT Project Coordinates to NAD 83 Values.

REVISED	STATE	STATE		SHEET NO.
		ROUTE	PROJECT	
	VA.	620	0620-029-017, P-101	IC

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

SURVEY DATA SHEET

SURVEY ALIGNMENTS					TRAVERSE POINTS					
POINT ID.	STATION	BEARING	PROJECT COORDINATES	ELEV. (Z)	DESCRIPTION	PT #	NORTH(Y)	EAST(X)	ELEV. (Z)	DESCRIPTION
SS	100+00.000	Rte. 609 Survey BL	441,62,479	3,567,467.932	-----	100	443,170.86	3,568,906.15	270.23'	8" NAIL
PI	109+84.250	N 25° 00' 49" E	442,054.414	3,567,884.106	283.14'	101	443,547.22	3,569,256.04	274.00'	8" NAIL
PI	119+15.580	N 44° 15' 22" E	442,721.458	3,568,534.050	266.16'	145	444,077.27	3,569,773.82	277.34'	8" NAIL
PI	136+86.870	N 42° 34' 46" E	444,025.729	3,569,732.526	277.17'	11	443,908.239	3,570,086.852	279.45'	8" NAIL
PI	146+55.460	N 43° 58' 46" E	444,722.716	3,570,405.115	288.86'	12	443,769.727	3,570,274.642	284.20'	8" NAIL
PI	151+57.000	N 48° 02' 22" E	445,058.055	3,570,778.063	287.04'	13	443,704.181	3,570,502.211	289.18'	8" NAIL
PI	156+56.820	N 2° 29' 28" W	445,557.403	3,570,756.339	287.21'	15	443,391.736	3,570,957.274	296.67'	8" NAIL

NOTE: The elevation given for ELK 1969 was to the nearest tenth of a meter (86.3m). For this project, the elevation was assumed to be 283.14' [86.300m].

RFP CONCEPTUAL PLANS

THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.

SUBMITTED BY:



IN ASSOCIATION WITH:



CONTROL STATION ID: ELK
 ROUTE 609 COUNTY/CITY FAIRFAX CO.
 ESTABLISHED BY U.G.S. YEAR 1969
 VERTICAL DATUM BASED ON NAVD 88 ELEV. 86.3m
 HORIZONTAL DATUM BASED ON NAD 83
 VA STATE PLANE COORDINATES
 NAD 1983 METRIC VALUES IN ZONE
 X (EAST) 3,567,428.005
 Y (NORTH) 2,134,730.371
 VA DEPT. TRAWS. PROJECT COORDINATES
 X (EAST) 3,567,884.106
 Y (NORTH) 442,054.414
 AZIMUTH TO STATION ----- IS -----

DETAILED DESCRIPTION

The station is a standard survey disk set in concrete, stamped ELK 1969. To reach station from Chantilly proceed west on U.S. Route 50, 2.7 mi. to left on Route 609, thence with same generally southwest 2.8 mi. to station on right. The station is at angle point in road, 0.5 mi. southwest of the intersection of Routes 609 and 620, 4 ft. southeast from an angle point in a fence.

LD-200 (REV. 8/2000)

Virginia Department of Transportation Horizontal Control

Control Station I. D. Trav. #100 Project 620-029-017
 Route 620 City/County Fairfax Date 02-06-2012
 Established By VDOT
 Vertical Datum Based On NAVD88 Geoid 03
 Horizontal Datum Based On NAD83 Zone North

VDOT Project Coordinates
 East (X) 3,568,906.15 ft.
 North (Y) 443,170.86 ft.
 Elevation 270.23' ft.

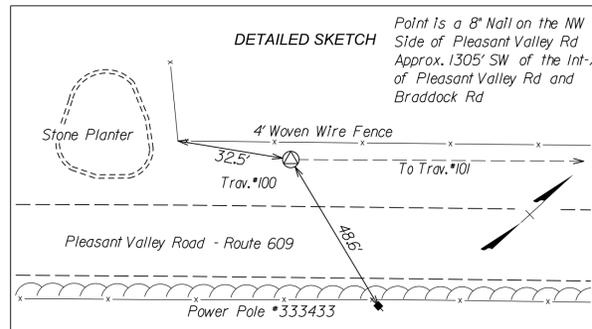
Latitude: 38° 52' 44.99835"
 Longitude: 77° 29' 19.70149"

Control Based On: GPS - Real Time Kinetic Network

Virginia State Plane Coordinates - NAD 83 Metric Values
 East (X) 3587739.5058 m
 North (Y) 2135070.6440 m
 Ortho. Elevation (H) 82.366 m

To convert state plane metric units to VDOT project values, use the following formula:
 1. Reduce the Easting Metric Values By 2.5 Million Meters. The South and North Zone Northing Metric Values By 1 and 2 Million Respectively.
 2. Multiply These Values by the U. S. Survey Foot (3.280833333)
 3. Multiply These Values by Combined Scale and Elevation Factor (1.00006) for the County.
 Reverse This Procedure to Transform VDOT Project Coordinates to NAD 83 Metric Plane Coordinates

• Sketch and Detailed Description Below •



LD-200 (REV. 8/2000)

Virginia Department of Transportation Horizontal Control

Control Station I. D. Trav. #101 Project 620-029-017
 Route 620 City/County Fairfax Date 02-06-2012
 Established By VDOT
 Vertical Datum Based On NAVD88 Geoid 03
 Horizontal Datum Based On NAD83 Zone North

VDOT Project Coordinates
 East (X) 3,569,256.04 ft.
 North (Y) 443,547.22 ft.
 Elevation 274.00' ft.

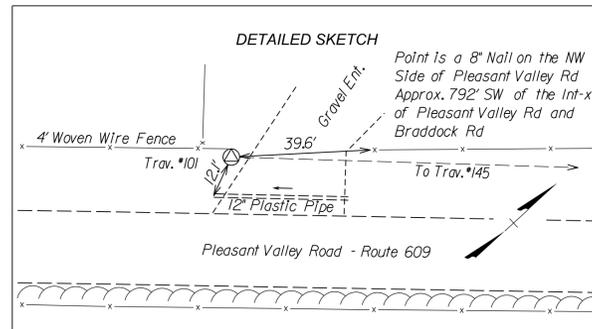
Latitude: 38° 52' 48.67998"
 Longitude: 77° 29' 15.22488"

Control Based On: GPS - Real Time Kinetic Network

Virginia State Plane Coordinates - NAD 83 Metric Values
 East (X) 3587846.1460 m
 North (Y) 2135185.3519 m
 Ortho. Elevation (H) 83.515 m

To convert state plane metric units to VDOT project values, use the following formula:
 1. Reduce the Easting Metric Values By 2.5 Million Meters. The South and North Zone Northing Metric Values By 1 and 2 Million Respectively.
 2. Multiply These Values by the U. S. Survey Foot (3.280833333)
 3. Multiply These Values by Combined Scale and Elevation Factor (1.00006) for the County.
 Reverse This Procedure to Transform VDOT Project Coordinates to NAD 83 Metric Plane Coordinates

• Sketch and Detailed Description Below •



LD-200 (REV. 8/2000)

Virginia Department of Transportation Horizontal Control

Control Station I. D. Trav. #13 Project 620-029-017
 Route 620 City/County Fairfax Date 02-06-2012
 Established By VDOT
 Vertical Datum Based On NAVD88 Geoid 03
 Horizontal Datum Based On NAD83 Zone North

VDOT Project Coordinates
 East (X) 3,570,502.211 ft.
 North (Y) 443,704.181 ft.
 Elevation 289.18' ft.

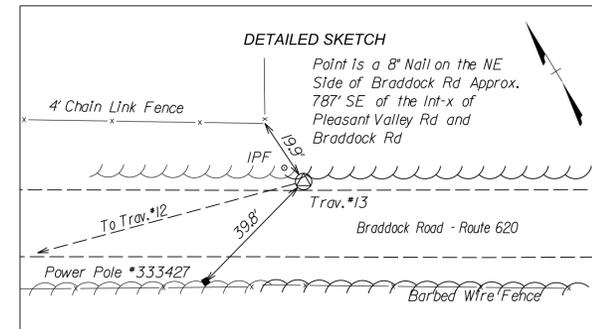
Latitude: 38° 52' 50.09517"
 Longitude: 77° 28' 59.44566"

Control Based On: GPS - Real Time Kinetic Network

Virginia State Plane Coordinates - NAD 83 Metric Values
 East (X) 3588225.9569 m
 North (Y) 2135233.1909 m
 Ortho. Elevation (H) 88.142 m

To convert state plane metric units to VDOT project values, use the following formula:
 1. Reduce the Easting Metric Values By 2.5 Million Meters. The South and North Zone Northing Metric Values By 1 and 2 Million Respectively.
 2. Multiply These Values by the U. S. Survey Foot (3.280833333)
 3. Multiply These Values by Combined Scale and Elevation Factor (1.00006) for the County.
 Reverse This Procedure to Transform VDOT Project Coordinates to NAD 83 Metric Plane Coordinates

• Sketch and Detailed Description Below •



LD-200 (REV. 8/2000)

Virginia Department of Transportation Horizontal Control

Control Station I. D. Trav. 15 Project 620-029-017
 Route 620 City/County Fairfax Date 02-06-2012
 Established By VDOT
 Vertical Datum Based On NAVD88 Geoid 03
 Horizontal Datum Based On NAD83 Zone North

VDOT Project Coordinates
 East (X) 3,570,957.274 ft.
 North (Y) 443,391.736 ft.
 Elevation 296.67' ft.

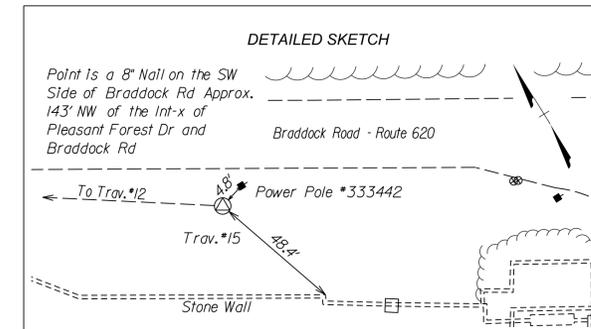
Latitude: 38° 52' 46.95727"
 Longitude: 77° 28' 53.73540"

Control Based On: GPS - Real Time Kinetic Network

Virginia State Plane Coordinates - NAD 83 Metric Values
 East (X) 3588364.6521 m
 North (Y) 2135137.9631 m
 Ortho. Elevation (H) 90.425 m

To convert state plane metric units to VDOT project values, use the following formula:
 1. Reduce the Easting Metric Values By 2.5 Million Meters. The South and North Zone Northing Metric Values By 1 and 2 Million Respectively.
 2. Multiply These Values by the U. S. Survey Foot (3.280833333)
 3. Multiply These Values by Combined Scale and Elevation Factor (1.00006) for the County.
 Reverse This Procedure to Transform VDOT Project Coordinates to NAD 83 Metric Plane Coordinates

• Sketch and Detailed Description Below •



PROJECT MANAGER: Amr. Salahsac. (703) 259-1957
SURVEYED BY, DATE: Leon Treutle, L.S. (V007) 12/03/2012
DESIGN BY: Timmons Group (804) 200-5500
SUBSURFACE UTILITY BY, DATE: Leon Treutle, L.S. (V007) 12/03/2012

HORIZONTAL ALIGNMENT DATA SHEET

REVISED	STATE	STATE		SHEET NO.
		ROUTE	PROJECT	
	VA.	620	0620-029-017, P-101	ID

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

ROUTE 609 CONSTRUCTION BASELINE

Chain RTE609 contains:
60901 CUR RTE609-1 CUR RTE609-2 CUR RTE609-3 CUR RTE609-4 CUR RTE609-5 CUR RTE-609-6 CUR RTE609-7 60902

Beginning chain RTE609 description

Point 60901 N 442,471.6441 E 3,568,284.0273 Sta 200+00.00

Course from 60901 to PC RTE609-1 N 43°06' 24.69" E Dist 1,277.2745

Curve Data			
Curve RTE609-1			
P.I. Station	213+23.98 N	443,438.2579 E	3,569,887.856
Delta	0° 32' 06.83" (RT)		
Degree	0° 34' 22.65"		
Tangent	46.7081		
Length	93.4156		
Radius	10,000.0000		
External	0.0091		
Long Chord	93.4152		
Mid.Ord.	0.0091		
P.C. Station	212+77.27 N	443,404.572 E	3,569,568.671
P.T. Station	213+70.69 N	443,472.0590 E	3,569,221.0213
C.C.	N 436,570.5454 E		3,576,457.6718
Back	N 43°06' 24.69" E		
Ahead	N 43°38' 31.53" E		
Chord Bear	N 43°22' 28.1" E		

Course from PT RTE609-1 to PC RTE609-2 N 43°38' 31.53" E Dist 377.4386

Curve Data			
Curve RTE609-2			
P.I. Station	218+60.40 N	443,826.4436 E	3,569,558.9940
Delta	1° 17' 11.26" (RT)		
Degree	0° 34' 22.65"		
Tangent	112.2696		
Length	224.5297		
Radius	10,000.0000		
External	0.6302		
Long Chord	224.5250		
Mid.Ord.	0.6302		
P.C. Station	217+48.13 N	443,745.981 E	3,569,481.5110
P.T. Station	219+72.66 N	443,905.9291 E	3,569,638.2815
C.C.	N 436,843.6845 E		3,576,718.1616
Back	N 43°38' 31.53" E		
Ahead	N 44°55' 42.78" E		
Chord Bear	N 44°17' 07.16" E		

Course from PT RTE609-2 to PC RTE609-3 N 44°55' 42.78" E Dist 55.5028

Curve Data			
Curve RTE609-3			
P.I. Station	220+71.93 N	443,976.2119 E	3,569,708.3892
Delta	19° 46' 59.10" (LT)		
Degree	22° 49' 37.21"		
Tangent	43.7683		
Length	86.6653		
Radius	251,000.0000		
External	3.7875		
Long Chord	86.2354		
Mid.Ord.	3.7312		
P.C. Station	220+28.16 N	443,945.2244 E	3,569,677.4789
P.T. Station	221+14.83 N	444,015.8324 E	3,569,726.9872
C.C.	N 444,122.4868 E		3,569,499.7739
Back	N 44°55' 42.78" E		
Ahead	N 25°08' 43.68" E		
Chord Bear	N 35°02' 13.23" E		

Curve Data			
Curve RTE609-4			
P.I. Station	221+45.78 N	444,043.8518 E	3,569,740.1396
Delta	14° 03' 36.77" (RT)		
Degree	22° 49' 37.21"		
Tangent	30.9528		
Length	61.5947		
Radius	251,000.0000		
External	1.9013		
Long Chord	61.4402		
Mid.Ord.	1.8870		
P.C. Station	221+14.83 N	444,015.8324 E	3,569,726.9872
P.T. Station	221+76.42 N	444,067.8366 E	3,569,759.7050
C.C.	N 443,909.1780 E		3,569,954.2004
Back	N 25°08' 43.68" E		
Ahead	N 39°12' 20.45" E		
Chord Bear	N 32°10' 32.07" E		

ROUTE 609 CONSTRUCTION BASELINE (CONT'D)

Chain RTE609 contains:
60901 CUR RTE609-1 CUR RTE609-2 CUR RTE609-3 CUR RTE609-4 CUR RTE609-5 CUR RTE-609-6 CUR RTE609-7 60902

Continue chain RTE609 description

Course from PT RTE609-4 to PC RTE609-5 N 39°12' 20.45" E Dist 95.0727

Curve Data			
Curve RTE609-5			
P.I. Station	222+87.89 N	444,154.2088 E	3,569,830.1628
Delta	7° 28' 23.37" (LT)		
Degree	22° 49' 37.21"		
Tangent	16.3924		
Length	32.7382		
Radius	251,000.0000		
External	0.5347		
Long Chord	32.7150		
Mid.Ord.	0.5336		
P.C. Station	222+71.49 N	444,141.5067 E	3,569,819.8010
P.T. Station	223+04.23 N	444,168.1508 E	3,569,838.7844
C.C.	N 444,300.1653 E		3,569,625.3057
Back	N 39°12' 20.45" E		
Ahead	N 31°43' 57.08" E		
Chord Bear	N 35°28' 08.77" E		

Curve Data			
Curve RTE609-6			
P.I. Station	223+63.70 N	444,218.7321 E	3,569,870.0637
Delta	26° 39' 34.26" (RT)		
Degree	22° 49' 37.21"		
Tangent	59.4716		
Length	116.7894		
Radius	251,000.0000		
External	6.9493		
Long Chord	117.3787		
Mid.Ord.	6.7621		
P.C. Station	223+04.23 N	444,168.1508 E	3,569,838.7844
P.T. Station	224+21.02 N	444,249.9014 E	3,569,920.7129
C.C.	N 444,036.1362 E		3,570,052.2631
Back	N 31°43' 57.08" E		
Ahead	N 58°23' 31.34" E		
Chord Bear	N 45°03' 44.21" E		

Course from PT RTE609-6 to PC RTE609-7 N 58°23' 31.34" E Dist 74.6526

Curve Data			
Curve RTE609-7			
P.I. Station	225+59.33 N	444,322.3915 E	3,570,038.5073
Delta	14° 13' 48.33" (LT)		
Degree	11° 14' 04.08"		
Tangent	63.6599		
Length	126.6646		
Radius	510,000.0000		
External	3.9578		
Long Chord	126.3393		
Mid.Ord.	3.9273		
P.C. Station	224+95.67 N	444,289.0271 E	3,569,984.2911
P.T. Station	226+22.34 N	444,368.0594 E	3,570,082.8584
C.C.	N 444,723.3707 E		3,569,716.9980
Back	N 58°23' 31.34" E		
Ahead	N 44°09' 43.01" E		
Chord Bear	N 51°16' 37.18" E		

Course from PT RTE609-7 to 60902 N 44°09' 43.01" E Dist 402.6615

Point 60902 N 444,656.9181 E 3,570,363.3882 Sta 230+25.00

Ending chain RTE609 description

ROUTE 609 AUX CONSTRUCTION BASELINE

Chain 609AUX contains:
AUX1 CUR 609AUX-1

Beginning chain 609AUX description

Point AUX1 N 443,924.7439 E 3,569,698.0106 Sta 220+28.16

Course from AUX1 to PC 609AUX-1 N 44°55' 42.78" E Dist 135.2113

Curve Data			
Curve 609AUX-13			
P.I. Station	221+95.87 N	444,043.4797 E	3,569,816.4506
Delta	64° 00' 24.59" (RT)		
Degree	110° 11' 03.09"		
Tangent	32.4975		
Length	58.0908		
Radius	52,000.0000		
External	9.3196		
Long Chord	55.1169		
Mid.Ord.	7.9031		
P.C. Station	221+63.37 N	444,020.4719 E	3,569,793.5001
P.T. Station	222+21.46 N	444,032.9342 E	3,569,847.1895
C.C.	N 443,983.7482 E		3,569,830.3155
Back	N 44°55' 42.78" E		
Ahead	S 71°03' 52.62" E		
Chord Bear	N 76°55' 55.08" E		

Ending chain 609AUX description

ROUTE 620 CONSTRUCTION BASELINE

Chain RTE620 contains:
62001 CUR RTE620-1 CUR RTE620-2 CUR RTE620-3 CUR RTE620-4 CUR RTE620-5 62002

Beginning chain RTE620 description

Point 62001 N 444,590.3573 E 3,568,988.1295 Sta 100+00.00

Course from 62001 to PC RTE620-1 S 59°39' 38.25" E Dist 422.0904

Curve Data			
Curve RTE620-8			
P.I. Station	105+75.45 N	444,299.6876 E	3,569,484.7673
Delta	1° 45' 25.88" (RT)		
Degree	0° 34' 22.65"		
Tangent	153.3556		
Length	306.6872		
Radius	10,000.0000		
External	1.1758		
Long Chord	306.6752		
Mid.Ord.	1.1757		
P.C. Station	104+22.09 N	444,377.1507 E	3,569,352.4140
P.T. Station	107+28.78 N	444,218.2024 E	3,569,614.6831
C.C.	N 435,746.6645 E		3,564,301.2053
Back	S 59°39' 38.25" E		
Ahead	S 57°54' 12.37" E		
Chord Bear	S 58°46' 55.31" E		

Course from PT RTE620-1 to PC RTE620-2 S 57°54' 12.37" E Dist 125.3526

Curve Data			
Curve RTE620-9			
P.I. Station	108+74.40 N	444,140.8270 E	3,569,738.0465
Delta	9° 13' 59.90" (RT)		
Degree	22° 49' 37.21"		
Tangent	20.2684		
Length	40.4491		
Radius	251,000.0000		
External	0.8170		
Long Chord	40.4053		
Mid.Ord.	0.8144		
P.C. Station	108+54.13 N	444,151.5966 E	3,569,720.8760
P.T. Station	108+94.58 N	444,127.4419 E	3,569,753.2664
C.C.	N 443,938.9610 E		3,569,587.5077
Back	S 57°54' 12.37" E		
Ahead	S 48°40' 12.46" E		
Chord Bear	S 53°17' 12.42" E		

Course from PT RTE620-2 to PC RTE620-3 S 48°40' 12.47" E Dist 49.7083

Curve Data			
Curve RTE620-10			
P.I. Station	109+83.21 N	444,068.9083 E	3,569,819.8238
Delta	17° 37' 51.05" (LT)		
Degree	22° 49' 37.21"		
Tangent	38.9260		
Length	77.2368		
Radius	251,000.0000		
External	3.0005		
Long Chord	76.9324		
Mid.Ord.	2.9650		
P.C. Station	109+44.29 N	444,094.6148 E	3,569,790.5934
P.T. Station	110+21.52 N	444,053.2627 E	3,569,855.4672
C.C.	N 444,283.0957 E		3,569,956.3521
Back	S 48°40' 12.46" E		
Ahead	S 66°18' 03.51" E		
Chord Bear	S 57°29' 07.99" E		

Course from PT RTE620-3 to PC RTE620-4 S 66°18' 03.51" E Dist 99.2013

Curve Data			
Curve RTE620-11			
P.I. Station	111+82.38 N	443,988.6096 E	3,570,002.7578
Delta	7° 03' 22.11" (RT)		
Degree	5° 43' 46.48"		
Tangent	61.6544		
Length	123.1529		
Radius	1,000.0000		
External	1.8988		
Long Chord	123.0751		
Mid.Ord.	1.8952		
P.C. Station	111+20.73 N	444,013.3905 E	3,569,946.3028
P.T. Station	112+43.88 N	443,957.0814 E	3,570,055.7412
C.C.	N 443,097.7210 E		3,569,544.3706
Back	S		

PROJECT MANAGER: Amr. Salahsoor (703) 259-1957
 SURVEYED BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012
 DESIGN BY: J. Jaramas Group (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012

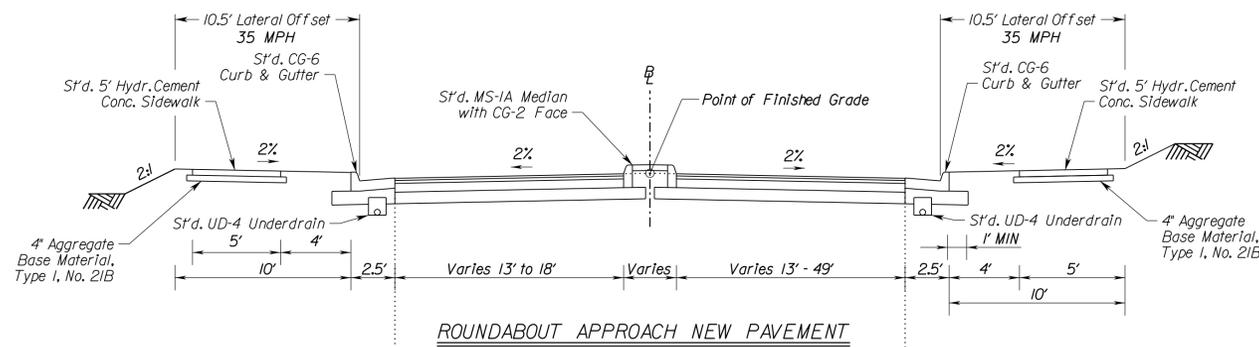
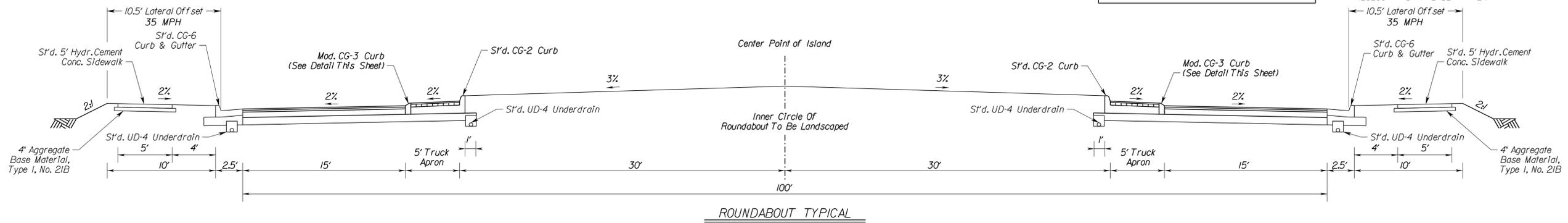
TYPICAL SECTIONS

RFP CONCEPTUAL PLANS

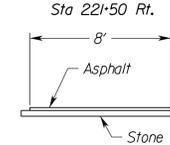
THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	620	0620-029-017, P-101	2

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

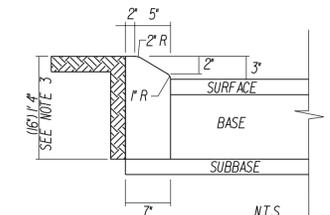


ALTERNATE ASPHALT PATH CONNECTOR



CG-3 MODIFIED

FOR USE ON ROUNDABOUT TRUCK APRONS ONLY

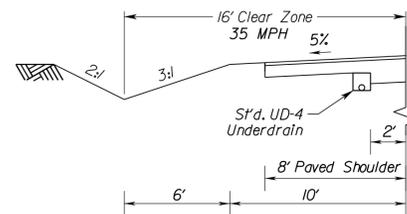


- NOTES:
1. THIS ITEM MAY BE PRECAST OR CAST IN PLACE.
 2. CONCRETE TO BE CLASS A3 IF CAST IN PLACE, 4000 PSI IF PRECAST.
 3. THE DEPTH OF CURB MAY BE REDUCED AS MUCH AS 3" (13' DEPTH) OR INCREASED AS MUCH AS 3" (19' DEPTH) IN ORDER THAT THE BOTTOM OF THE CURB WILL COINCIDE WITH THE TOP OF A COURSE OF THE PAVEMENT'S SUBSTRUCTURE. OTHERWISE, THE DEPTH IS TO BE 16" AS SHOWN, NO ADJUSTMENT IN THE PRICE BID IS TO BE MADE FOR A DECREASE OR AN INCREASE IN DEPTH.
 4. THE MODIFICATION TO THE STANDARD CG-3 IS TO REDUCE THE EXPOSED HEIGHT OF THE CURB AS SHOWN. MODIFIED CURB SHALL BE PAID FOR AS STANDARD CG-3.

SHOULDER SECTION LEFT

ROUTE 609
Sta 224+89 to Sta 227+34

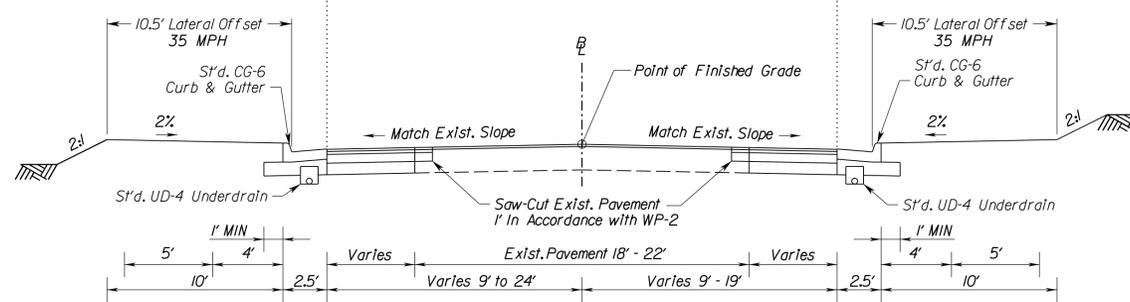
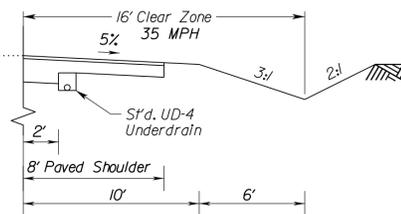
ROUTE 620
Sta 102+83 to Sta 107+55
Sta 110+94 to Sta 113+39



SHOULDER SECTION RIGHT

ROUTE 609
Sta 217+23 to Sta 219+68

ROUTE 620
Sta 110+94 to Sta 113+39

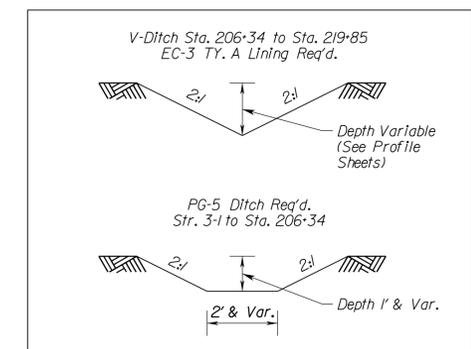


ROUNDABOUT APPROACH OVERLAY AND WIDENING

ROUTE 609
Sta 217+22.66 to Sta 219+71.62
Sta 226+00 to Sta 227+33.79

ROUTE 620
Sta 102+83 to Sta 108+04
Sta 112+00 to Sta 113+39.36

TYPICAL OUTFALL DITCH SECTION



PROJECT MANAGER: Amir Salasoooc (703) 259-1957
 SURVEYED BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012
 DESIGN BY: J. Timmons Group (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012

NOTE: ADDITIONAL UTILITY EASEMENTS MAY BE REQUIRED.

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	620	0620-029-017, P-101	3

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

RFP CONCEPTUAL PLANS

THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.

SUBMITTED BY:



FORT MYER CONSTRUCTION
Since 1972

IN ASSOCIATION WITH:



WBCM
ARCHITECTURE, ENGINEERING, CONSTRUCTION

Braddock Road
Fairfax County, VA
Utility Owners List

Water:
Fairfax Water Authority
8570 Executive Park Ave
Fairfax, VA 22031
Michael Jenkins (703) 698 - 5600

COX-RICHARD FAMILY FARM LLLP
DB21045 PG23
93,999 AC.
Tax# 043-1-01-0019-Z

Power:
Northern Virginia Electric Coop.
14500 Minnieville Road
Woodbridge, VA 22193-0459
(703) 392 - 1519

Telecomm:
Verizon (VZN)
22001 Loudoun County Pkwy
Ashburn, VA 20147
Steve Puryear (703) 886 - 6490

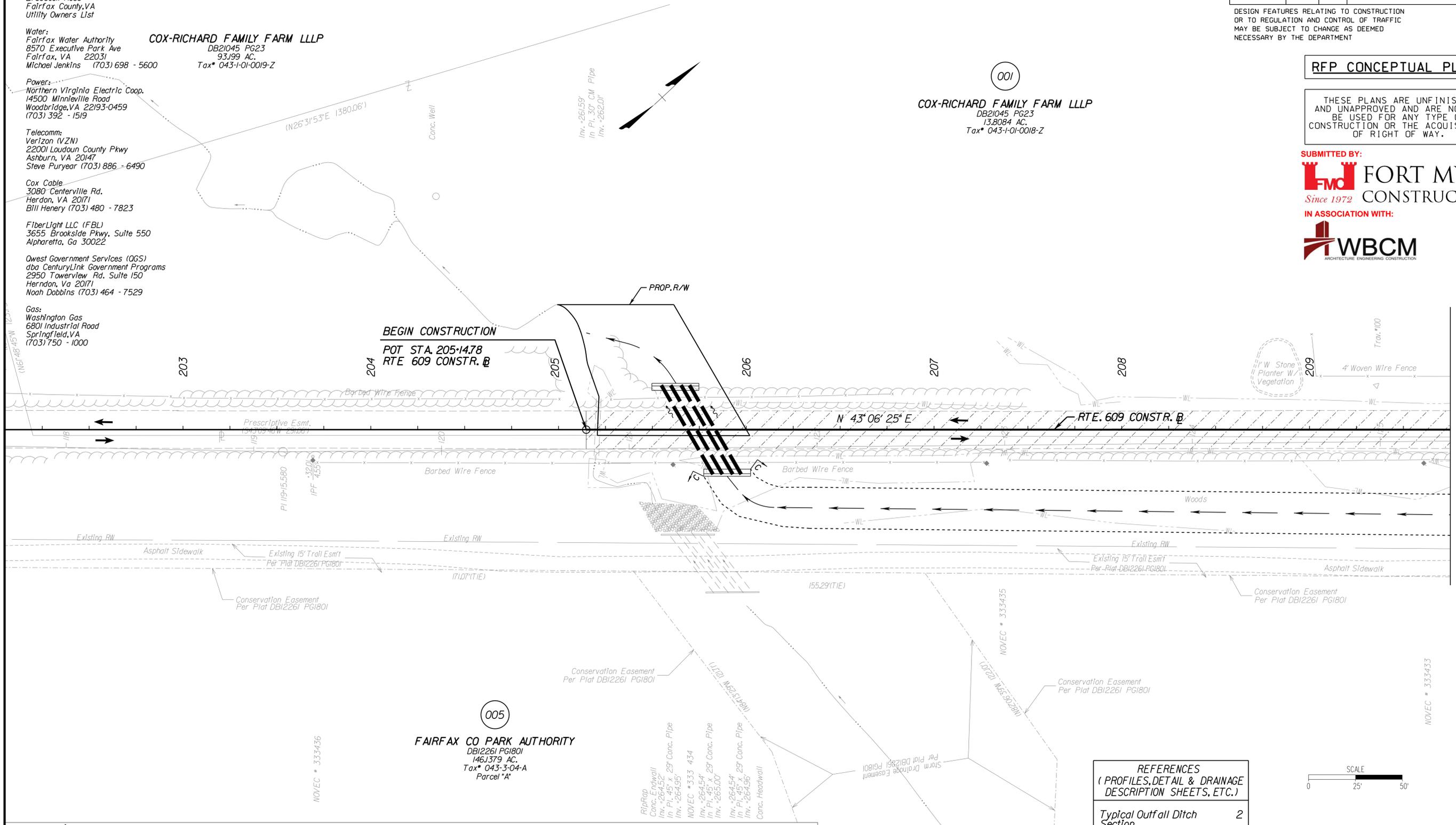
Cox Cable
3080 Centerville Rd.
Herdon, VA 20171
Bill Henery (703) 480 - 7823

FiberLight LLC (FBL)
3655 Brookside Pkwy, Suite 550
Alpharetta, Ga 30022

Owest Government Services (OGS)
dba CenturyLink Government Programs
2950 Towerview Rd, Suite 150
Herdon, Va 20171
Noah Dobbins (703) 464 - 7529

Gas:
Washington Gas
6801 Industrial Road
Springfield, VA
(703) 750 - 1000

001
COX-RICHARD FAMILY FARM LLLP
DB21045 PG23
13,808.4 AC.
Tax# 043-1-01-0018-Z



005
FAIRFAX CO PARK AUTHORITY
DB12261 PG1801
146,137.9 AC.
Tax# 043-3-04-A
Parcel 'A'

LEGEND

- 1. S'd. MS-1 Median Req'd.
- 2. S'd. MS-1A Median Req'd.
- 3. S'd. CG-6 Curb & Gutter Req'd.
- 4. S'd. CG-2 Curb Req'd.
- 5. Mod. CG-3 Curb Req'd. (See Detail Sheet IB)
- 6. S'd. CG-12 Type A Req'd.
- 7. S'd. CG-12 Type B Req'd.
- 8. S'd. 5' Hydr. Cement Conc. Sidewalk Req'd.
- 9. Prop. 8' Paved Shoulder
- 10. Prop. Concrete Truck Apron
- 11. Prop. 8' Asphalt/Stone Multi-Use Path Req'd.
- 12. S'd. CG-12 Type M Req'd.

Denotes Prop. V-Notch Ditch

Denotes Construction Limits In Cuts

Denotes Construction Limits In Fills

Denotes New Pavement

Denotes Pavement Resurfacing

Denotes Demolition of Pavement

Denotes Prop. Truck Apron

Denotes 8' Asphalt Stone Multi-Use Path

REFERENCES
(PROFILES, DETAIL & DRAINAGE DESCRIPTION SHEETS, ETC.)

Typical Outfall Ditch Section	2
Route 609 Profile	3A



MATCHLINE SHEET 4 - STA. 209+75.00

PROJECT	0620-029-017	SHEET NO.	3
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PROJECT MANAGER _ _ Amir Salasoor (703) 259-1957 _ _ _ _ _
 SURVEYED BY, DATE _ Leon Icaulte, L.S.(VDOT)12/03/2012 _ _ _ _ _
 DESIGN BY _ Timmons Group (804) 200-6500 _ _ _ _ _
 SUBSURFACE UTILITY BY, DATE _ Leon Icaulte, L.S.(VDOT)12/03/2012 _ _ _ _ _

REVISED	STATE	ROUTE	STATE	PROJECT	SHEET NO.
	VA.	620		0620-029-017, P-101	3A

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

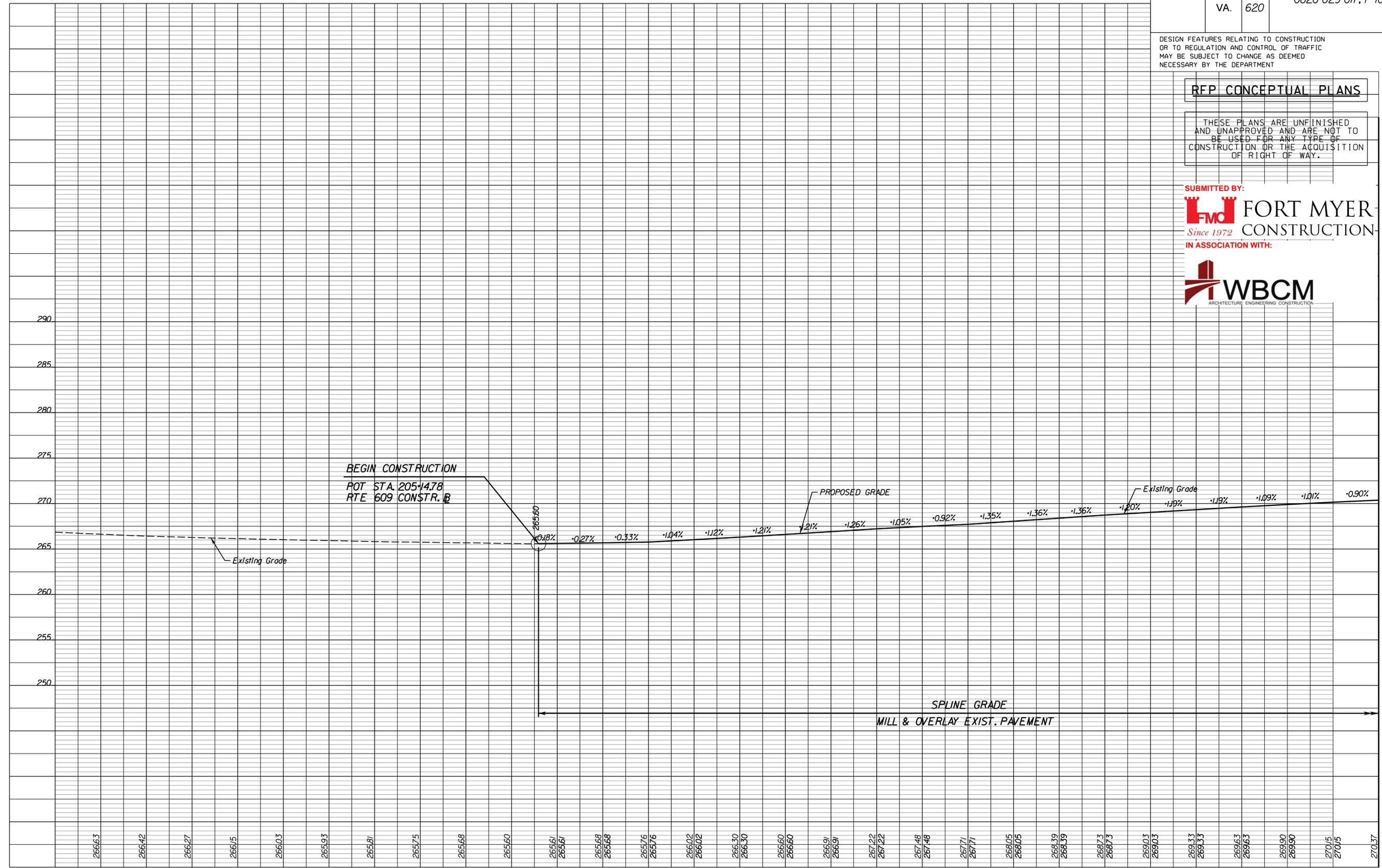
REF CONCEPTUAL PLANS

THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.

SUBMITTED BY:



IN ASSOCIATION WITH:

BEGIN CONSTRUCTION
 POT STA. 205+47.8
 RTE 609 CONSTR. B

PROPOSED GRADE

Existing Grade

SPLINE GRADE
 MILL & OVERLAY EXIST. PAVEMENT

MATCHLINE SHEET 4A - STA. 209+75.00

26663 26642 26627 26615 26603 26593 26581 26575 26568 26560 26551 26561 26568 26568 26576 26576 26602 26602 26630 26630 26660 26660 26691 26691 26722 26722 26748 26748 26771 26771 26805 26805 26839 26839 26873 26873 26903 26903 26933 26933 26963 26963 26990 26990 27015 27015 27037

203+00.00 204+00.00 205+00.00 206+00.00 207+00.00 208+00.00 209+00.00

PROJECT MANAGER: Amir, Salahsoor (703) 259-1957
 SURVEYED BY, DATE: Leon, Treutle, L.S. (VDOT) 12/03/2012
 DESIGN BY: J. J. Woods, Geop. (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leon, Treutle, L.S. (VDOT) 12/03/2012

NOTE: ADDITIONAL UTILITY EASEMENTS MAY BE REQUIRED.

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	620	0620-029-017, P-101	4

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

RFP CONCEPTUAL PLANS

THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.

SUBMITTED BY:



IN ASSOCIATION WITH:



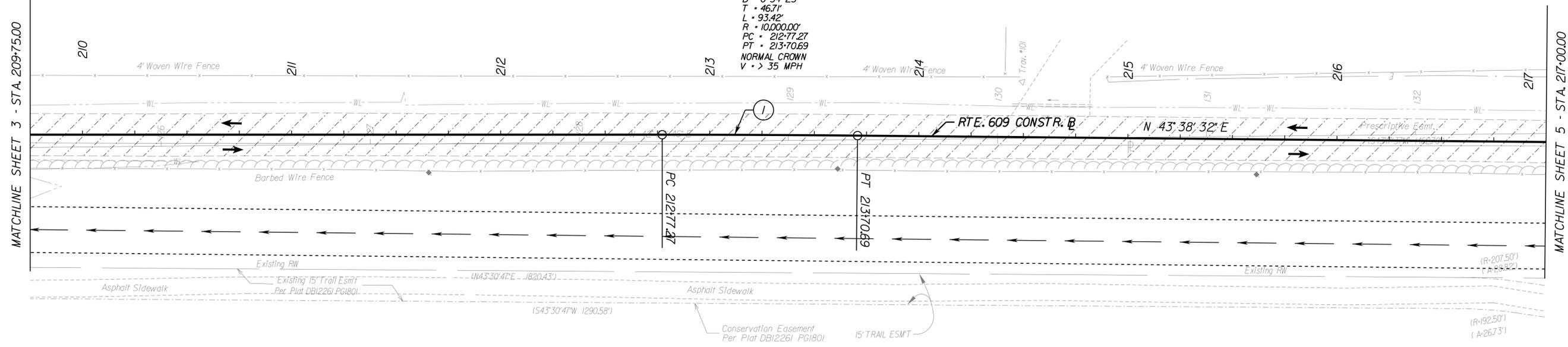
001

COX-RICHARD FAMILY FARM LLLP
 DB21045 PG23
 13.8084 AC.
 Tax# 043-1-01-0018-Z

Gravel Ent.
 In Pl. 12" Plastic Pipe
 Inv. In* 272.80'
 Inv. Out* 272.50'

1

Curve RTE609-1
 PI - 213+23.98
 DELTA - 0° 32' 06.83" (RT)
 D - 0° 34' 23"
 T - 46.71'
 L - 93.42'
 R - 10,000.00'
 PC - 212+77.27
 PT - 213+70.69
 NORMAL CROWN
 V - > 35 MPH



005

FAIRFAX CO PARK AUTHORITY
 DB12261 PG1801
 146.1379 AC.
 Tax# 043-3-04-A
 Parcel "A"

NOVEC # 333437

NOVEC # 333432

NOVEC # 433411

LEGEND

- 1. S'd. MS-1 Median Req'd.
- 2. S'd. MS-1A Median Req'd.
- 3. S'd. CG-6 Curb & Gutter Req'd.
- 4. S'd. CG-2 Curb Req'd.
- 5. Mod. CG-3 Curb Req'd. (See Detail Sheet IB)
- 6. S'd. CG-12 Type A Req'd.
- 7. S'd. CG-12 Type B Req'd.
- 8. S'd. 5' Hydr. Cement Conc. Sidewalk Req'd.
- 9. Prop. 8' Paved Shoulder
- 10. Prop. Concrete Truck Apron
- 11. Prop. 8' Asphalt/Stone Multi-Use Path Req'd.
- 12. S'd. CG-12 Type M Req'd.

- Denotes Prop. V-Notch Ditch
- [C] Denotes Construction Limits In Cuts
- [E] Denotes Construction Limits In Fills

- [Dotted] Denotes New Pavement
- [Diagonal Lines] Denotes Pavement Resurfacing
- [Cross-hatch] Denotes Prop. Truck Apron
- [Horizontal Lines] Denotes 8' Asphalt Stone Multi-Use Path
- [Diagonal Lines] Denotes Demolition of Pavement

REFERENCES
 (PROFILES, DETAIL & DRAINAGE DESCRIPTION SHEETS, ETC.)

Typical Outfall Ditch Section	2
Route 609 Profile	4A



PROJECT	0620-029-017	SHEET NO.	4
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PROJECT MANAGER _ _ Amir Salasoor (703) 259-1957 _ _ _ _ _
 SURVEYED BY, DATE _ Leon I ceutle, L.S.VDOT\12\03\2012 _ _ _ _ _
 DESIGN BY _ Timmons Group (804) 200-6500 _ _ _ _ _
 SUBSURFACE UTILITY BY, DATE _ Leon I ceutle, L.S.VDOT\12\03\2012 _ _ _ _ _

REVISED	STATE	ROUTE	STATE	PROJECT	SHEET NO.
	VA.	620		0620-029-017, P-101	4A

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

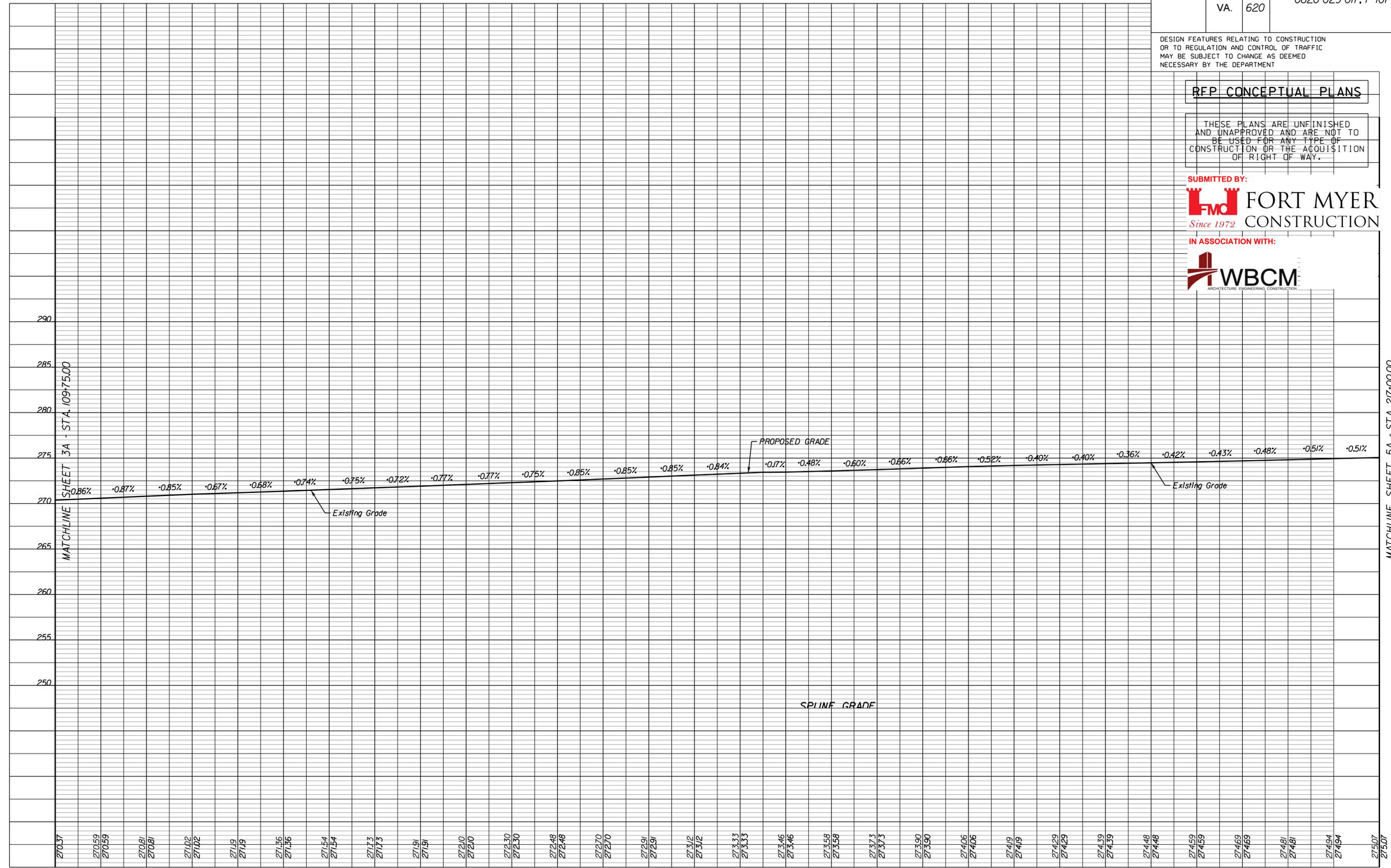
REP CONCEPTUAL PLANS

THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.

SUBMITTED BY:



IN ASSOCIATION WITH:



MATCHLINE SHEET 3A - STA. 109+75.00

MATCHLINE SHEET 5A - STA. 217+00.00

270.37 270.59 270.59 270.81 270.81 271.02 271.02 271.19 271.19 271.36 271.36 271.54 271.54 271.73 271.73 271.91 271.91 272.10 272.10 272.30 272.30 272.48 272.48 272.70 272.70 272.91 272.91 273.12 273.12 273.33 273.33 273.46 273.46 273.58 273.58 273.73 273.73 273.90 273.90 274.06 274.06 274.19 274.19 274.29 274.29 274.39 274.39 274.48 274.48 274.59 274.59 274.69 274.69 274.81 274.81 274.94 274.94 275.07 275.07

210+00.00 211+00.00 212+00.00 213+00.00 214+00.00 215+00.00 216+00.00 217+00.00

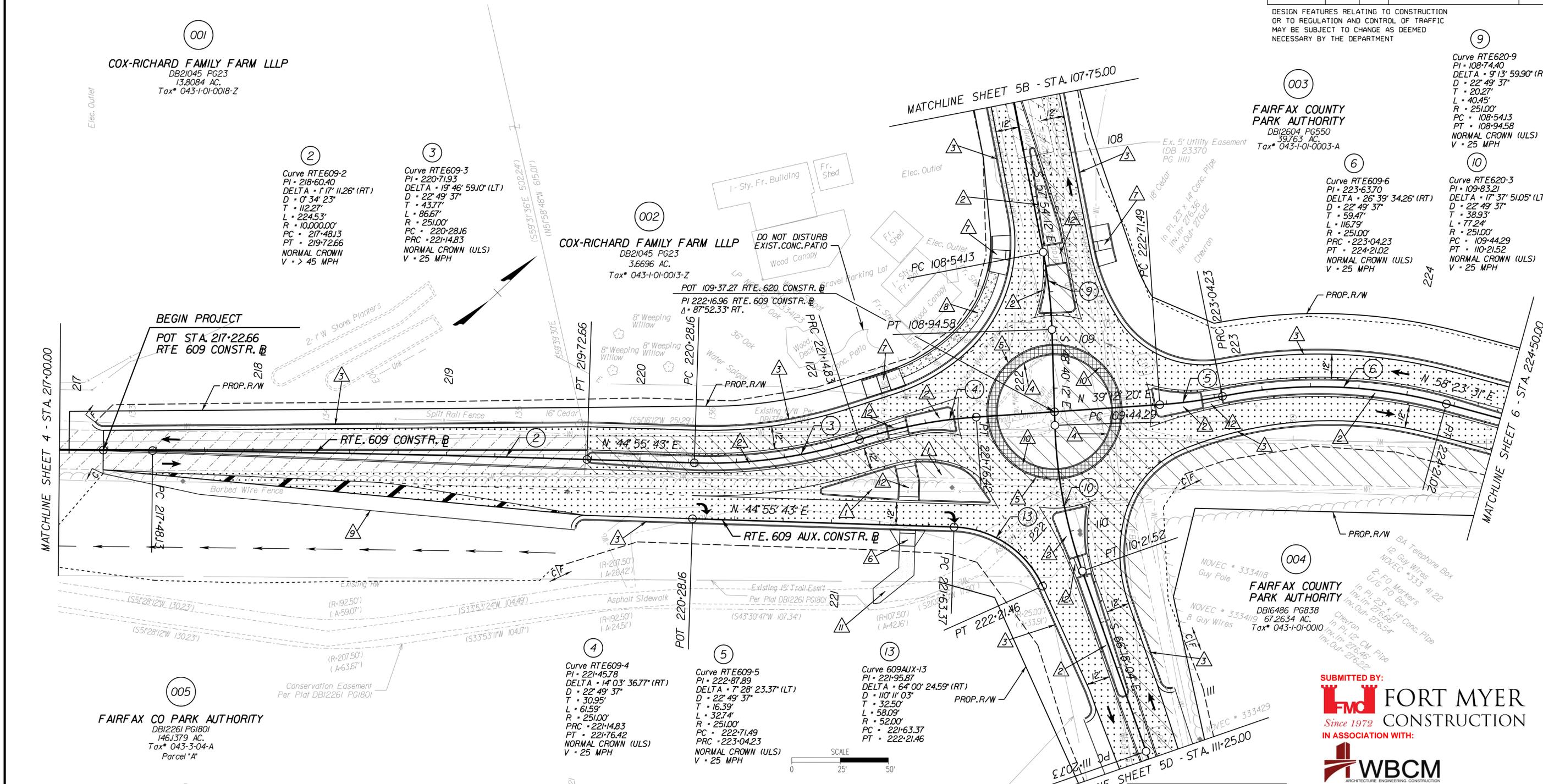
PROJECT	0620-029-107	SHEET NO.	4A
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PROJECT MANAGER: Amir, Salahsoor, (703) 259-1957
SURVEYED BY, DATE: Leon, Treutle, L.S., (VDOT) 12/03/2012
DESIGN BY: J. Timmons, Group, (804) 200-6500
SUBSURFACE UTILITY BY, DATE: Leon, Treutle, L.S., (VDOT) 12/03/2012

NOTE: ADDITIONAL UTILITY EASEMENTS MAY BE REQUIRED.

REVISED	STATE	ROUTE	STATE	PROJECT	SHEET NO.
	VA.	620		0620-029-017, P-101	5

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



001
COX-RICHARD FAMILY FARM LLLP
DB21045 PG23
13,808.4 AC.
Tax* 043-1-01-0018-Z

2
Curve RTE609-2
PI - 218+60.40
DELTA - 17° 11' 26" (RT)
D - 0' 34' 23"
T - 112.27'
L - 224.53'
R - 10,000.00'
PC - 217+48.13
PT - 219+72.66
NORMAL CROWN (ULS)
V - > 45 MPH

3
Curve RTE609-3
PI - 220+71.93
DELTA - 19° 46' 59.0" (LT)
D - 23' 49' 37"
T - 43.77'
L - 86.67'
R - 251.00'
PC - 220+28.16
PT - 221+4.83
NORMAL CROWN (ULS)
V - 25 MPH

002
COX-RICHARD FAMILY FARM LLLP
DB21045 PG23
3,669.6 AC.
Tax* 043-1-01-0013-Z

POT 109+37.27 RTE. 620 CONSTR. B
PI 222+16.96 RTE. 609 CONSTR. B
Δ - 87° 52.33' RT.

003
FAIRFAX COUNTY
PARK AUTHORITY
DBI2604 PG550
39,763 AC.
Tax* 043-1-01-0003-A

6
Curve RTE609-6
PI - 223+63.70
DELTA - 26° 39' 34.26" (RT)
D - 22' 49' 37"
T - 59.47'
L - 116.79'
R - 251.00'
PC - 223+04.23
PT - 224+21.02
NORMAL CROWN (ULS)
V - 25 MPH

9
Curve RTE620-9
PI - 108+74.40
DELTA - 9° 13' 59.90" (RT)
D - 22' 49' 37"
T - 20.27'
L - 40.45'
R - 251.00'
PC - 108+54.13
PT - 108+94.58
NORMAL CROWN (ULS)
V - 25 MPH

10
Curve RTE620-3
PI - 109+83.21
DELTA - 17° 37' 51.05" (LT)
D - 22' 49' 37"
T - 38.93'
L - 77.24'
R - 251.00'
PC - 109+44.29
PT - 110+21.52
NORMAL CROWN (ULS)
V - 25 MPH

BEGIN PROJECT
POT STA. 217+22.66
RTE 609 CONSTR. B

MATCHLINE SHEET 4 - STA. 217+00.00

MATCHLINE SHEET 6 - STA. 224+50.00

005
FAIRFAX CO PARK AUTHORITY
DBI2261 PG1801
146,137.9 AC.
Tax* 043-3-04-A
Parcel "A"

4
Curve RTE609-4
PI - 221+45.78
DELTA - 14° 03' 36.77" (RT)
D - 22' 49' 37"
T - 30.95'
L - 61.59'
R - 251.00'
PC - 221+4.83
PT - 221+76.42
NORMAL CROWN (ULS)
V - 25 MPH

5
Curve RTE609-5
PI - 222+87.89
DELTA - 7° 28' 23.37" (LT)
D - 22' 49' 37"
T - 16.39'
L - 32.74'
R - 251.00'
PC - 222+71.49
PT - 223+04.23
NORMAL CROWN (ULS)
V - 25 MPH

13
Curve 609AUX-13
PI - 221+95.87
DELTA - 64° 00' 24.59" (RT)
D - 110' 11' 03"
T - 32.50'
L - 58.09'
R - 52.00'
PC - 221+63.37
PT - 222+21.46

004
FAIRFAX COUNTY
PARK AUTHORITY
DBI6486 PG838
67,263.4 AC.
Tax* 043-1-01-0010



LEGEND

1. S'd. MS-1 Median Req'd.	8. S'd. 5' Hydr. Cement Conc. Sidewalk Req'd.		Denotes New Pavement		Denotes Prop. Truck Apron
2. S'd. MS-1A Median Req'd.	9. Prop. 8' Paved Shoulder		Denotes Pavement Resurfacing		Denotes 8' Asphalt Stone Multi-Use Path
3. S'd. CG-6 Curb & Gutter Req'd.	10. Prop. Concrete Truck Apron		Denotes Demolition of Pavement		
4. S'd. CG-2 Curb Req'd.	11. Prop. 8' Asphalt/Stone Multi-Use Path Req'd.				
5. Mod. CG-3 Curb Req'd. (See Detail Sheet IB)	12. S'd. CG-12 Type M Req'd.				
6. S'd. CG-12 Type A Req'd.			Denotes Prop. V-Notch Ditch		
7. S'd. CG-12 Type B Req'd.			Denotes Construction Limits In Cuts		
			Denotes Construction Limits In Fills		

REFERENCES
(PROFILES, DETAIL & DRAINAGE DESCRIPTION SHEETS, ETC.)

Typical Outfall Ditch Section	2
Route 609 Profile	5A
Route 620 Profile	5A

SUBMITTED BY:
FMC FORT MYER CONSTRUCTION
Since 1972
IN ASSOCIATION WITH:



RFP CONCEPTUAL PLANS

THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.

PROJECT	SHEET NO.
0620-029-017	5

PROJECT MANAGER - Amir Salahpour (703) 253-1957
 SURVEYED BY, DATE - Leon Icaulte, L.S. (VDOT) 12/03/2012
 DESIGN BY - Timmons Group (804) 200-6500
 SUBSURFACE UTILITY BY, DATE - Leon Icaulte, L.S. (VDOT) 12/03/2012

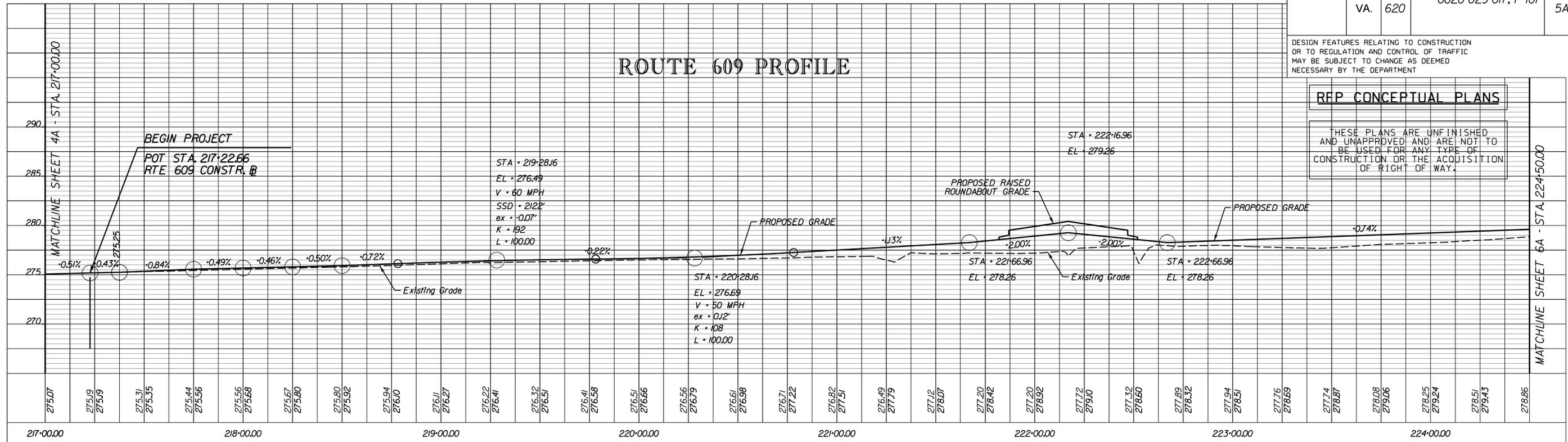
REVISED	STATE	ROUTE	STATE	PROJECT	SHEET NO.
	VA.	620		0620-029-017, P-101	5A

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

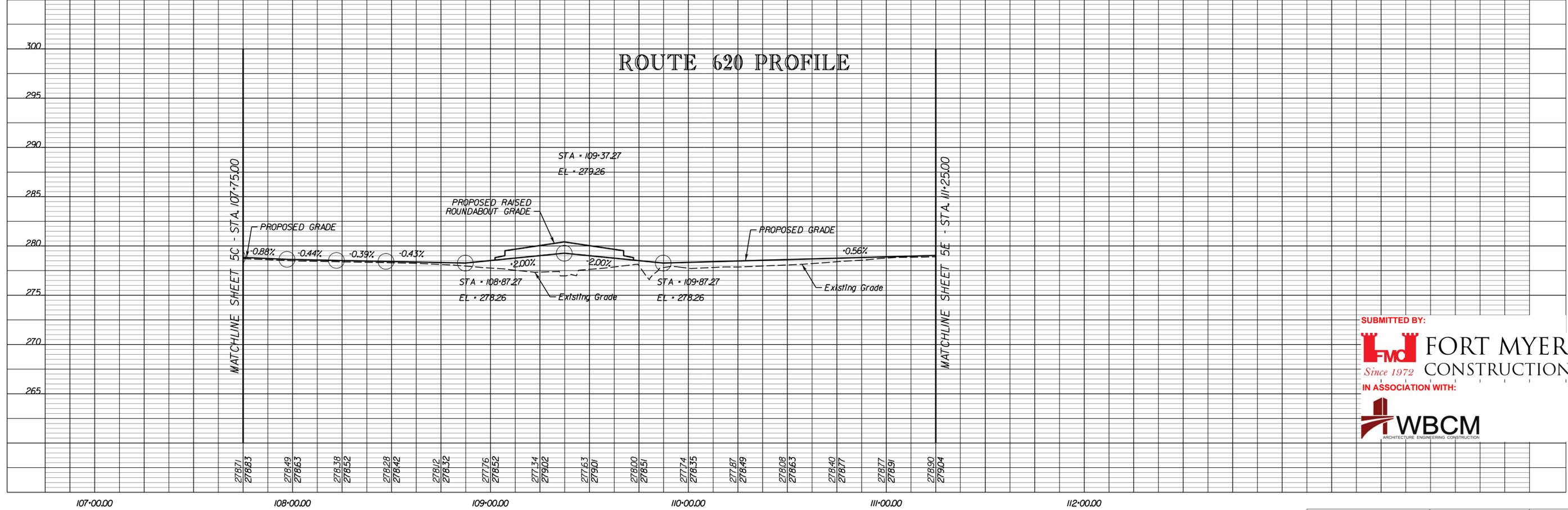
RFP CONCEPTUAL PLANS

THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.

ROUTE 609 PROFILE



ROUTE 620 PROFILE



SUBMITTED BY:



FORT MYER CONSTRUCTION
 Since 1972

IN ASSOCIATION WITH:



WBCM
 ARCHITECTURE ENGINEERING CONSTRUCTION

PROJECT MANAGER: Amr. Salahsoor (703) 259-1957
SURVEYED BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012
DESIGN BY: J. J. Woods, Group (804) 200-6500
SUBSURFACE UTILITY BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012

NOTE: ADDITIONAL UTILITY EASEMENTS MAY BE REQUIRED.

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	620	0620-029-017, P-101	5B

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

RFP CONCEPTUAL PLANS

THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.

SUBMITTED BY:



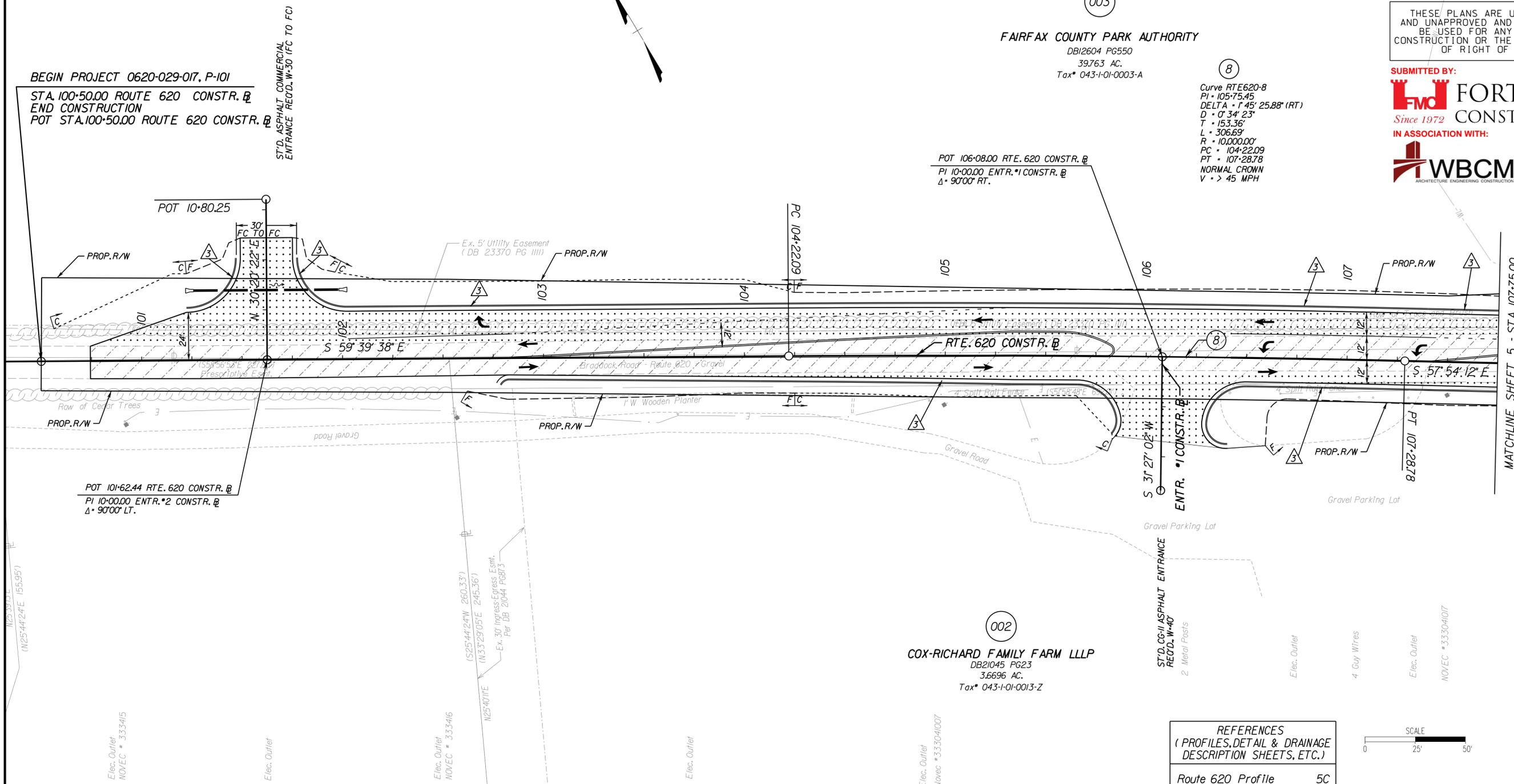
IN ASSOCIATION WITH:



FAIRFAX COUNTY PARK AUTHORITY
DBI2604 PG550
39,763 AC.
Tax# 043-1-01-0003-A

(8)
Curve RTE620-8
PI = 105+75.45
DELTA = 7° 45' 25.88" (RT)
D = 0° 34' 23"
T = 153.36'
L = 306.69'
R = 10,000.00'
PC = 104+22.09
PT = 107+28.78
NORMAL CROWN
V = > 45 MPH

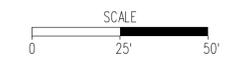
BEGIN PROJECT 0620-029-017, P-101
STA. 100+50.00 ROUTE 620 CONSTR. B
END CONSTRUCTION
POT STA. 100+50.00 ROUTE 620 CONSTR. B



COX-RICHARD FAMILY FARM LLLP
DB21045 PG23
3,669.6 AC.
Tax# 043-1-01-0013-Z

REFERENCES
(PROFILES, DETAIL & DRAINAGE DESCRIPTION SHEETS, ETC.)

Route 620 Profile	5C
Drainage Desc.	7
Entrance Profile	5C



LEGEND

1. S'd. MS-1 Median Req'd.	8. S'd. 5' Hydr. Cement Conc. Sidewalk Req'd.		Denotes New Pavement		Denotes Prop. Truck Apron
2. S'd. MS-1A Median Req'd.	9. Prop. 8' Paved Shoulder		Denotes Pavement Resurfacing		Denotes 8' Asphalt Stone Multi-Use Path
3. S'd. CG-6 Curb & Gutter Req'd.	10. Prop. Concrete Truck Apron		Denotes Demolition of Pavement		
4. S'd. CG-2 Curb Req'd.	11. Prop. 8' Asphalt/Stone Multi-Use Path Req'd.		Denotes Prop. V-Notch Ditch		
5. Mod. CG-3 Curb Req'd. (See Detail Sheet IB)	12. S'd. CG-12 Type M Req'd.		Denotes Construction Limits In Cuts		
6. S'd. CG-12 Type A Req'd.			Denotes Construction Limits In Fills		
7. S'd. CG-12 Type B Req'd.					

MATCHLINE SHEET 5 - STA. 107+75.00

PROJECT MANAGER: Amir Salasoor (703) 259-1957
 SURVEYED BY, DATE: Leon Icaulte, L.S. (VDOT) 12/03/2012
 DESIGN BY: Timmons Group (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leon Icaulte, L.S. (VDOT) 12/03/2012

RFP CONCEPTUAL PLANS

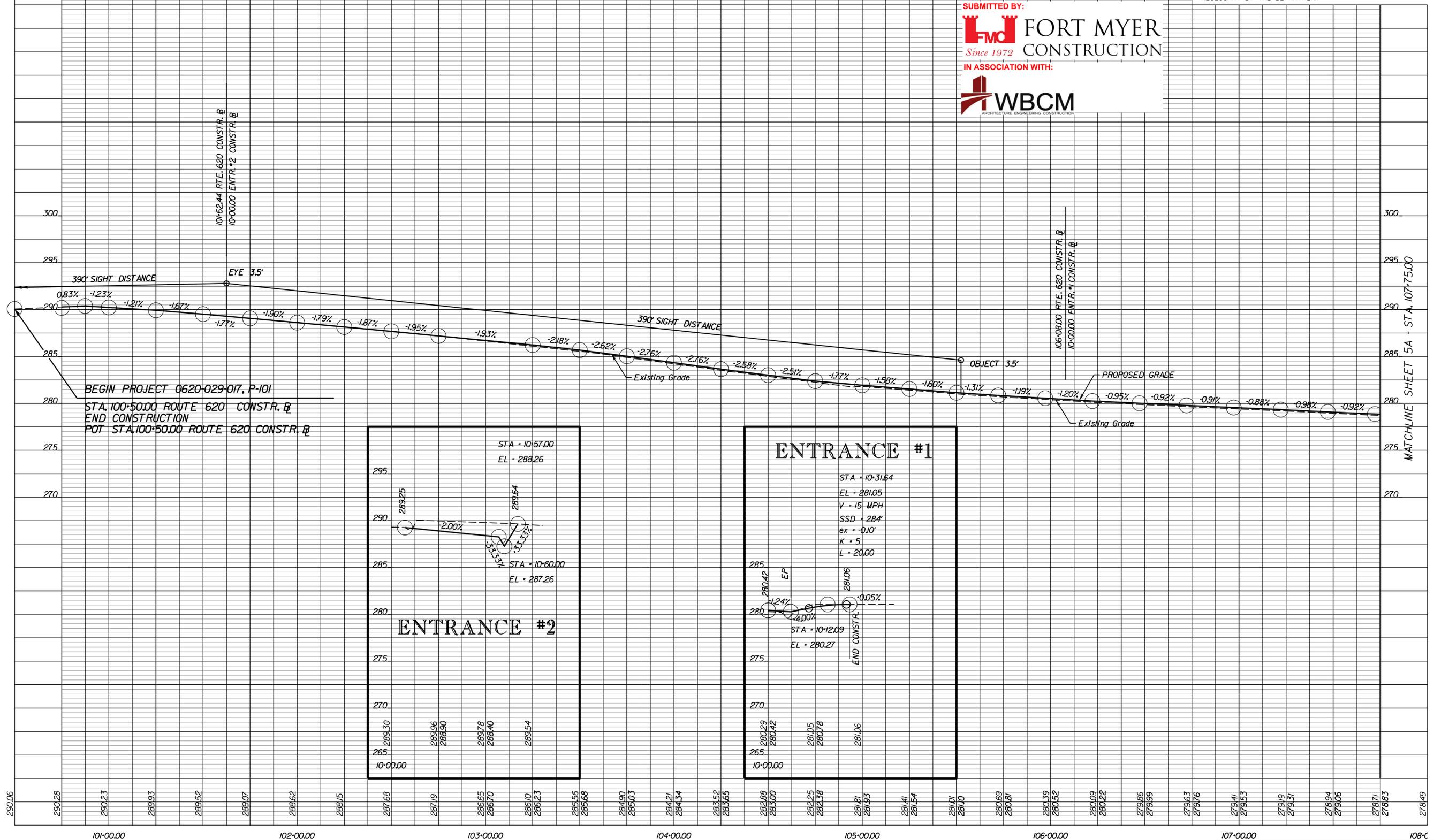
THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.



REVISED	STATE	ROUTE	STATE	PROJECT	SHEET NO.
	VA.	620		0620-029-017, P-101	5C

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

RTE. 620 PROFILE



PROJECT MANAGER: Amir, Salahsoor (703) 259-1957
 SURVEYED BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012
 DESIGN BY: J. J. Woods Group (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012

NOTE: ADDITIONAL UTILITY EASEMENTS MAY BE REQUIRED.

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	620	0620-029-017, P-101	5D

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

RFP CONCEPTUAL PLANS

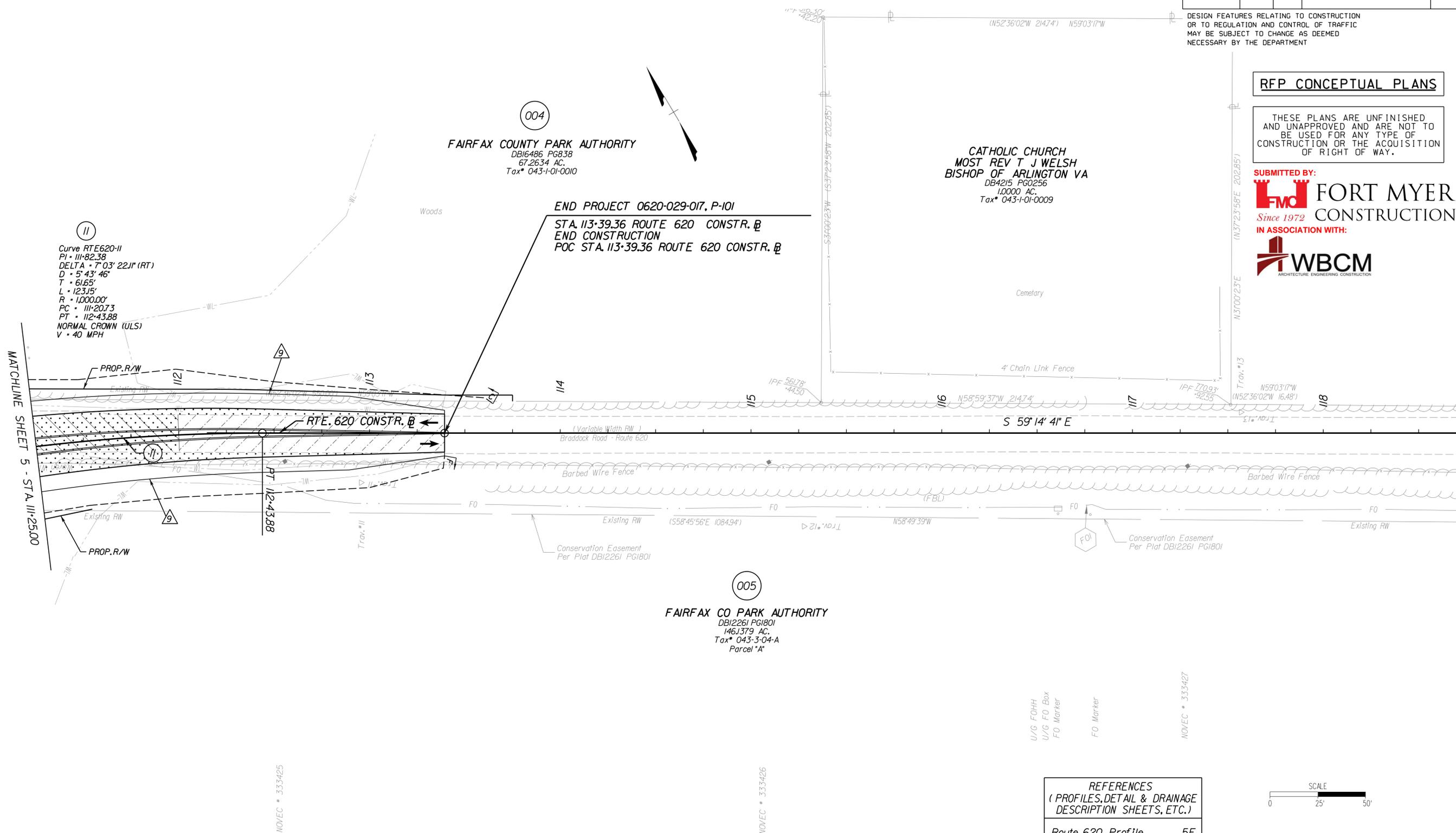
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SUBMITTED BY:

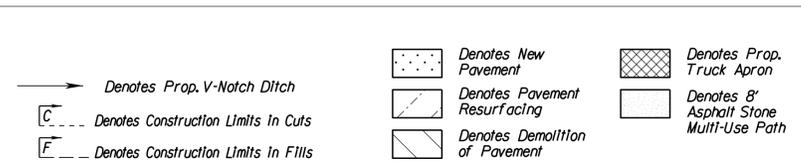
FMD FORT MYER CONSTRUCTION
 Since 1972

IN ASSOCIATION WITH:

WBCM
 ARCHITECTURE ENGINEERING CONSTRUCTION

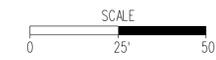


- LEGEND**
- 1. S'd. MS-1 Median Req'd.
 - 2. S'd. MS-1A Median Req'd.
 - 3. S'd. CG-6 Curb & Gutter Req'd.
 - 4. S'd. CG-2 Curb Req'd.
 - 5. Mod. CG-3 Curb Req'd. (See Detail Sheet IB)
 - 6. S'd. CG-12 Type A Req'd.
 - 7. S'd. CG-12 Type B Req'd.
 - 8. S'd. 5' Hydr. Cement Conc. Sidewalk Req'd.
 - 9. Prop. 8' Paved Shoulder
 - 10. Prop. Concrete Truck Apron
 - 11. Prop. 8' Asphalt/Stone Multi-Use Path Req'd.
 - 12. S'd. CG-12 Type M Req'd.



REFERENCES
 (PROFILES, DETAIL & DRAINAGE DESCRIPTION SHEETS, ETC.)

Route 620 Profile 5E



PROJECT	SHEET NO.
0620-029-017	5D

PROJECT MANAGER _ _ Amir Salasoor (703) 259-1957 _ _ _ _ _
 SURVEYED BY, DATE _ Leon Icaulte, L.S. (VDOT) 12/03/2012 _ _ _ _ _
 DESIGN BY _ Timmons Group (804) 200-6500 _ _ _ _ _
 SUBSURFACE UTILITY BY, DATE _ Leon Icaulte, L.S. (VDOT) 12/03/2012 _ _ _ _ _

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	620	0620-029-017, P-101	5E

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

RFP CONCEPTUAL PLANS

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SUBMITTED BY:



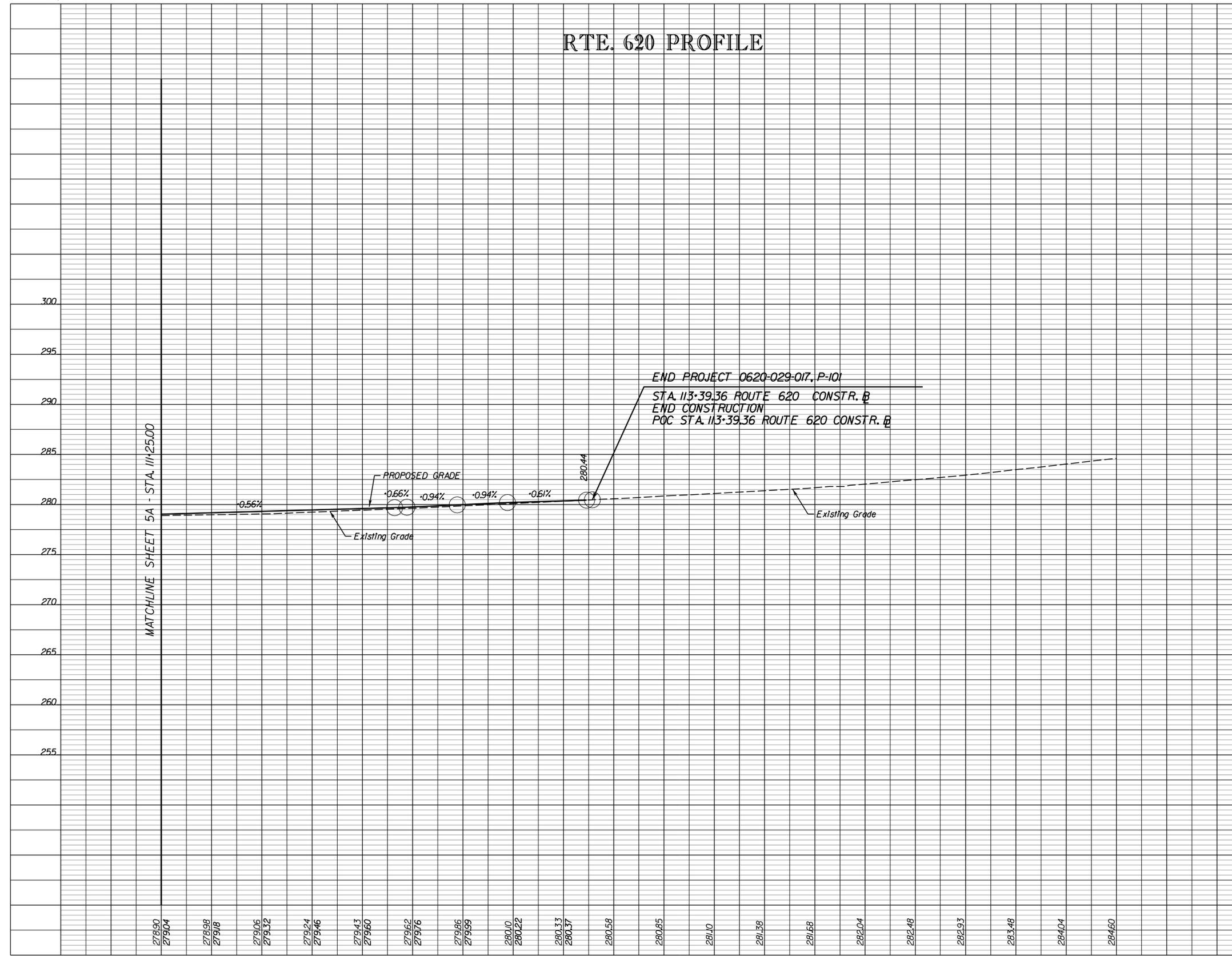
FORT MYER CONSTRUCTION
Since 1972

IN ASSOCIATION WITH:



WBCM
ARCHITECTURE ENGINEERING CONSTRUCTION

RTE. 620 PROFILE



111+00.00 112+00.00 113+00.00 114+00.00 115+00.00 116+00.00

278.90
279.04

278.98
279.16

279.06
279.32

279.24
279.46

279.43
279.60

279.62
279.76

279.86
279.99

280.10
280.22

280.33
280.37

280.58

280.85

281.10

281.38

281.68

282.04

282.48

282.93

283.48

284.04

284.60

PROJECT MANAGER: Amr. Salahsoor (703) 259-1957
 SURVEYED BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012
 DESIGN BY: J. Jaramas Group (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012

NOTE: ADDITIONAL UTILITY EASEMENTS MAY BE REQUIRED.

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	620	0620-029-017, P-101	6

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

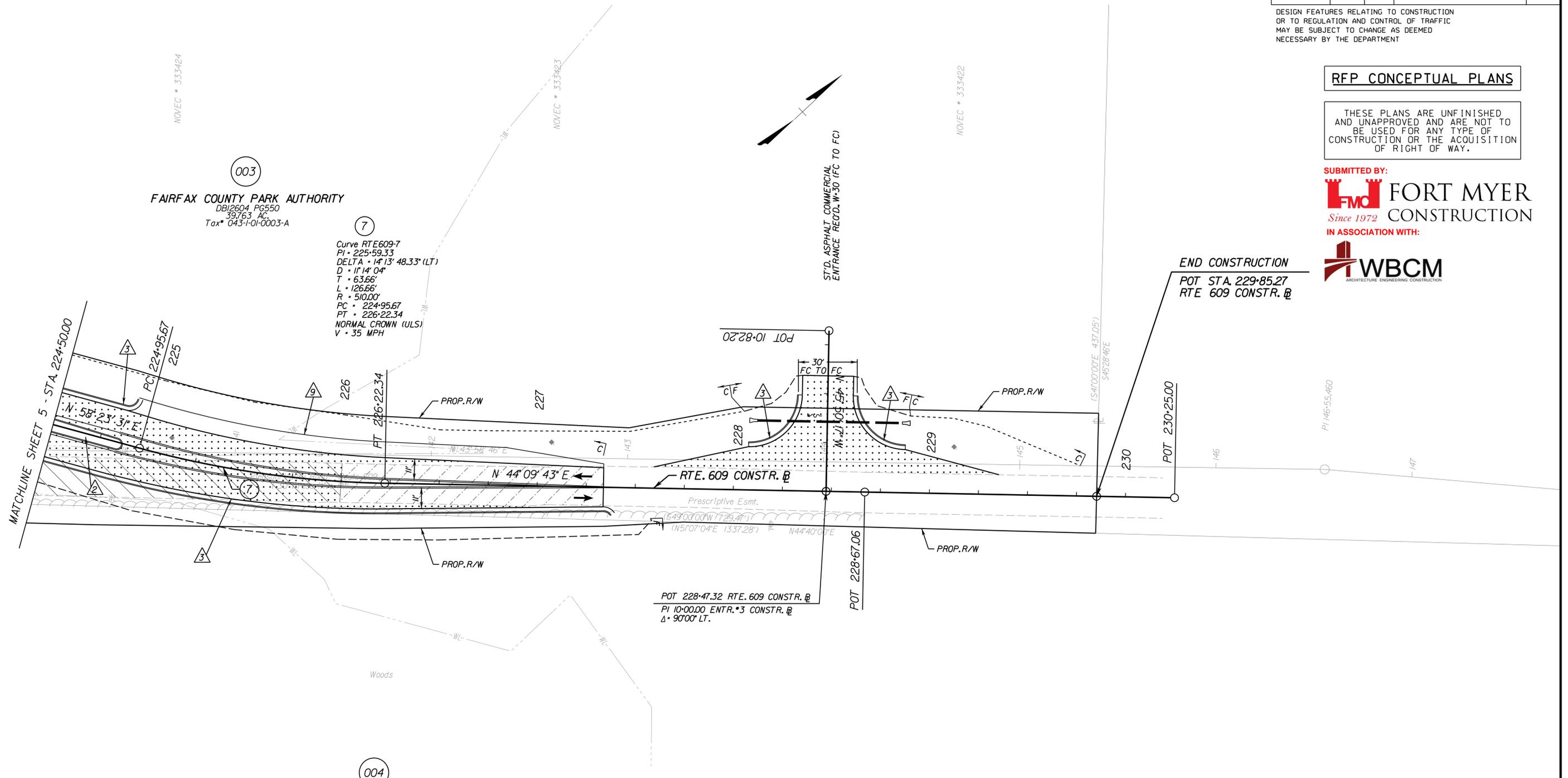
RFP CONCEPTUAL PLANS

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SUBMITTED BY:



IN ASSOCIATION WITH:



FAIRFAX COUNTY PARK AUTHORITY
 DB16486 PGB38
 67,2634 AC.
 Tax# 043-1-01-0010

REFERENCES
 (PROFILES, DETAIL & DRAINAGE DESCRIPTION SHEETS, ETC.)



Route 609 Profile 6A

LEGEND	
1. S'd. MS-1 Median Req'd.	8. S'd. 5' Hydr. Cement Conc. Sidewalk Req'd.
2. S'd. MS-1A Median Req'd.	9. Prop. 8' Paved Shoulder
3. S'd. CG-6 Curb & Gutter Req'd.	10. Prop. Concrete Truck Apron
4. S'd. CG-2 Curb Req'd.	11. Prop. 8' Asphalt/Stone Multi-Use Path Req'd.
5. Mod. CG-3 Curb Req'd. (See Detail Sheet 1B)	12. S'd. CG-12 Type M Req'd.
6. S'd. CG-12 Type A Req'd.	
7. S'd. CG-12 Type B Req'd.	
Denotes Prop. V-Notch Ditch	Denotes New Pavement
Denotes Construction Limits In Cuts	Denotes Pavement Resurfacing
Denotes Construction Limits In Fills	Denotes Prop. Truck Apron
	Denotes 8' Asphalt Stone Multi-Use Path
	Denotes Demolition of Pavement

PROJECT MANAGER _ Amir Salasoor (703) 259-1957 _
 SURVEYED BY, DATE _ Leon Icaulte, L.S. (VDOT) 12/03/2012 _
 DESIGN BY _ Timmons Group (804) 200-6500 _
 SUBSURFACE UTILITY BY, DATE _ Leon Icaulte, L.S. (VDOT) 12/03/2012 _

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	620	0620-029-017, P-101	6A

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

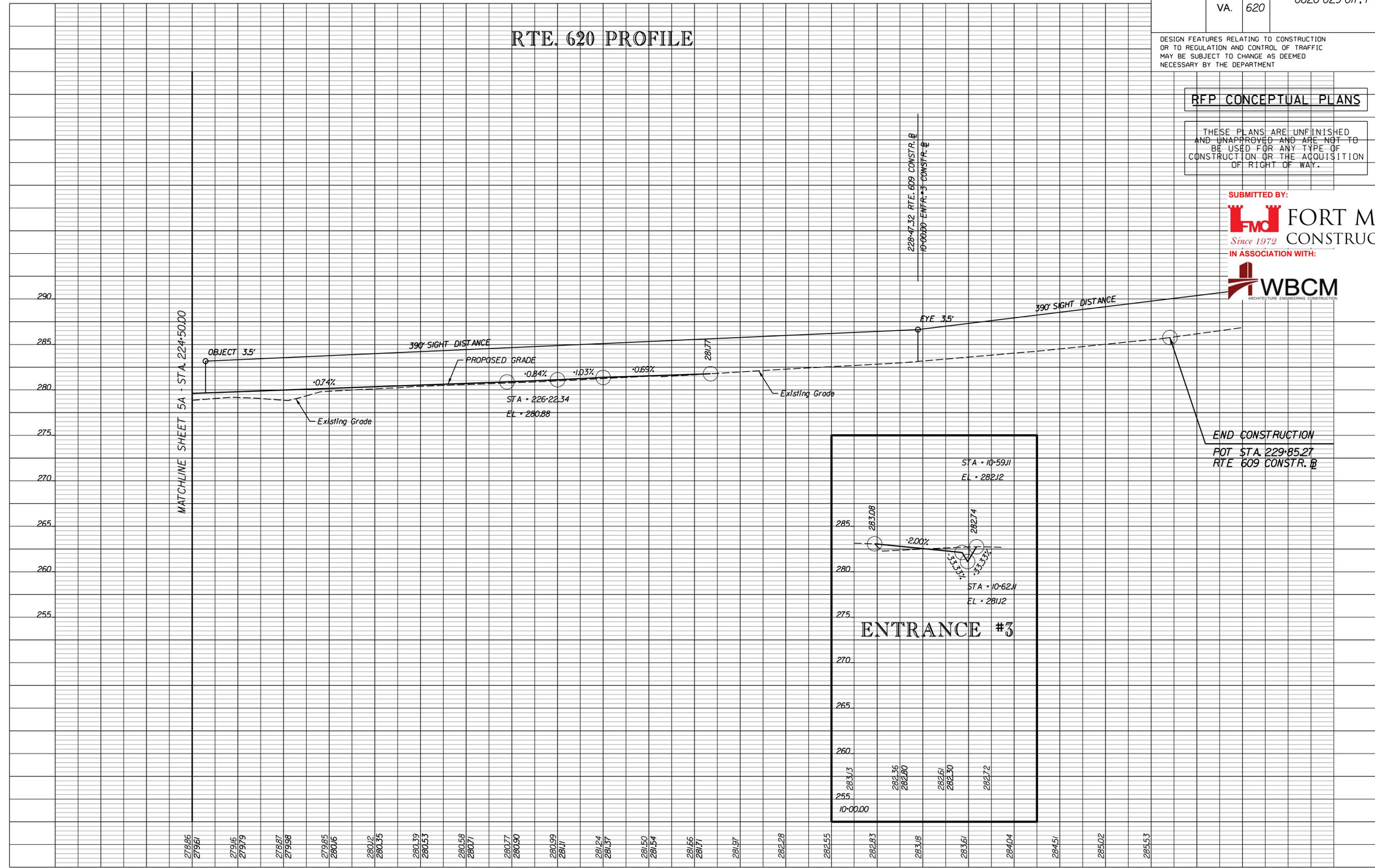
RFP CONCEPTUAL PLANS

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SUBMITTED BY:



RTE. 620 PROFILE



END CONSTRUCTION
 POT STA. 229+85.27
 RTE 609 CONSTR. @

PROJECT MANAGER: Amr, Salahsoor, (703) 259-1957
 SURVEYED BY: Leoa Treutle, LS, (VDOT) 12/03/2012
 DESIGNED BY: Timmans Group, (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leoa Treutle, LS, (VDOT) 12/03/2012

GENERAL NOTES

REVISED	STATE	STATE		SHEET NO.
	ROUTE	PROJECT		
	VA.	620	0620-029-017, P-101	8(1)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

RFP CONCEPTUAL PLANS

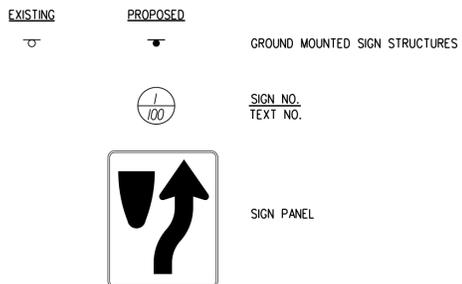
THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.

DEFINITION OF TYPES

TYPE	DESCRIPTION	SIZE
DP-1	O/H Double Pole	50 - 75 L. F.
DP-2	O/H Double Pole	76 - 101 L. F.
DP-3	O/H Double Pole	102 - 127 L. F.
DP-4	O/H Double Pole	128 - 153 L. F.
DP-5	O/H Double Pole	154 - 179 L. F.
DP-6	O/H Double Pole	180 - 205 L. F.
DC-1	O/H Double Pole & Cantilever	75 - 100 L. F.
DC-2	O/H Double Pole & Cantilever	101 - 126 L. F.
DC-3	O/H Double Pole & Cantilever	127 - 152 L. F.
DC-4	O/H Double Pole & Cantilever	153 - 178 L. F.
DC-5	O/H Double Pole & Cantilever	179 - 204 L. F.
DC-6	O/H Double Pole & Cantilever	205 - 230 L. F.
CS-1	O/H Single Arm Cantilever	25 - 40 L. F.
CS-2	O/H Single Arm Cantilever	41 - 60 L. F.
CD-1	O/H Double Arm Cantilever	50 - 74 L. F.
CD-2	O/H Double Arm Cantilever	75 - 125 L. F.
CD-3	O/H Double Arm Cantilever	126 - 175 L. F.
CD-4	O/H Double Arm Cantilever	176 - 225 L. F.
BM	O/H Bridge Mount	EA
I	Non-breakaway Single Metal Pole	EA
II	Non-breakaway Two Metal Poles	EA
III	Non-breakaway Three Metal Poles	EA
IV	Breakaway Single Round Metal Pole	EA
V	Breakaway Single Metal Pole	EA
VI	Breakaway Two Metal Poles	EA
VII	Breakaway Three Metal Poles	EA
VIII	3 1/2" Rolled Rail Steel Pole	EA
WP-1	Single Wood Post	EA
WP-2	Two Wood Posts	EA
SP-1	Sign Panel	0 - 100 S. F.
SP-2	Sign Panel	101 - 200 S. F.
SP-3	Sign Panel	201 - 300 S. F.
SP-4	Sign Panel	301 - 400 S. F.
SP-5	Sign Panel	401 - 500 S. F.
SP-6	Sign Panel	501 - 600 S. F.
STP-1	Square Tube Sign Post	EA

- Unless otherwise approved by the Engineer, existing traffic signs which are to be relocated shall remain in place until the new sign structure is in place.
 - The removal or modification of existing sign panels, structures, or foundations shall conform to section 510 of the specifications.
 - New materials and items required to complete the removal or modification of existing items shall be submitted to the Engineer for review and approval in accordance with section 105 of the specifications.
 - All existing and proposed sign locations are approximate and shall be field verified by the contractor. All proposed sign locations shall be according to VDOT standards and shall be approved by the Engineer.
 - All striping, where matching to existing, shall be done in a manner approved by the Engineer.
6. Definition of Symbols:
- EXAMPLE :
- SP-1 Sign Panel 0-100 S.F.
Q Relocate existing sign panel, Type ()
- USP-1 U - Type Steel Post EA.
G Remove and dispose of sign structure, Type ()
- Raised pavement markers shall be installed according to the standard details.
 - Existing pavement markings and/or markers that conflict with the proposed markings and/or markers shown herein shall be eradicated.
 - For lane and shoulder widths, see Roadway Plans and Typical Sections.
 - Proposed signs and pavement marking shall be according to VDOT standards.
 - Cost of Class A3 concrete foundation required for wood posts shall be included with the cost of the wood post.
 - Existing signs to remain in place unless otherwise noted on plans.

SIGNING LEGEND

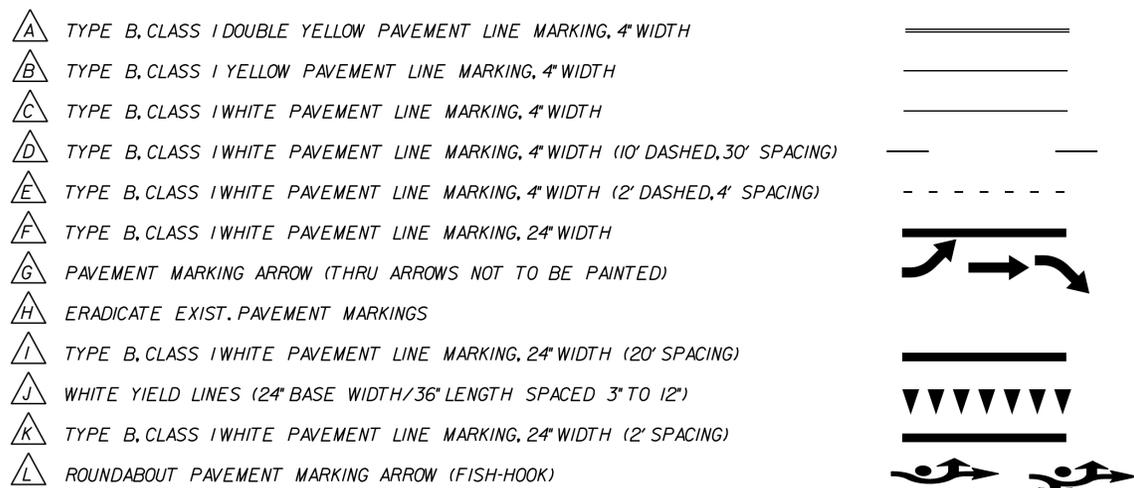


* ACTION DESCRIPTIONS

- G. REMOVE AND DISPOSE OF SIGN STRUCTURE, TYPE (), will be measured in units of each and paid for at the contract unit price per each, which price shall be full compensation for removal and disposal of sign panels, posts and foundations to at least two feet below existing ground line, backfilling and restoration (topsoiling and seeding), and for all materials, labor, tools, equipment and incidentals necessary to complete the work.
- H. REMOVE AND SALVAGE SIGN STRUCTURE, TYPE (), will be measured in units of each and paid for at the contract unit price per each, which price shall be full compensation for removal and salvage of sign panels, posts and foundations to at least two feet below existing ground line, backfilling and restoration (topsoiling and seeding), and for all materials, labor, tools, equipment and incidentals necessary to complete the work.
- Q. RELOCATE EXISTING SIGN PANEL, TYPE (), will be measured in units of each and paid for at the contract unit price per each, which price shall be full compensation for removing existing panel, refurbishing framing members, furnishing and installing necessary back panels, erecting existing sign panel to new breakaway posts, and for all materials, labor, tools, equipment and incidentals necessary to complete the work.
- S. REMOVE AND DISPOSE OF EXISTING SIGN PANEL, TYPE (), will be measured in units of each and paid for at the contract unit price per each, which price shall be full compensation for removing and disposing of sign panels and for all materials, labor, tools, equipment and incidentals necessary to complete the work.

*The cost of all action descriptions listed above shall be included in lump sum contract price.

PAVEMENT MARKING LEGEND



SUBMITTED BY:



IN ASSOCIATION WITH:



PROJECT	0620-029-017	SHEET NO.	8(1)
---------	--------------	-----------	------

PROJECT MANAGER: Amir Salahsoor (703) 259-1957
 SURVEYED BY: Leo J. Reulle, L.S. (VDOT) 12/03/2012
 DESIGNED BY: Timmons Group (804) 200-6500
 SUBSURFACE UTILITY BY DATE: Leo J. Reulle, L.S. (VDOT) 12/03/2012

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	620	0620-029-017, P-101	8(2)

SIGN SCHEDULE

TEXT NO.	SIGN ASSEMBLY NO(s).	TEXT	MUTCD ST'D.	SIGN ASSEMBLY COMPONENTS		QTY.	SIGN PANEL AREA (s.f.)		PROP. SIGN STRUCTURE ST'D.	REMARKS
				W	H		PER ASSEMBLY	ALL ASSEMBLIES		
1	101		R1-2	36"	36"	8	9	72	STP-1	
2	102		R4-7	36"	48"	4	12	48	STP-1	
3	103		W11-2	30"	30"	5	6.3	31.5	STP-1	
			W16-7P	24"	12"	5	2	10		
4	104		W11-2	30"	30"	5	6.3	31.5	STP-1	
			W16-7P	24"	12"	5	2	10		
5	105		R6-4	30"	24"	4	5	20	STP-1	
6	106		W2-6	30"	30"	4	6.3	25.2	STP-1	

TEXT NO.	SIGN ASSEMBLY NO(s).	TEXT	MUTCD ST'D.	SIGN ASSEMBLY COMPONENTS		QTY.	SIGN PANEL AREA (s.f.)		PROP. SIGN STRUCTURE ST'D.	REMARKS
				W	H		PER ASSEMBLY	ALL ASSEMBLIES		
7	107		R3-8	36"	36"	1	9	9	STP-1	

NOTES:

- 1) ALL SIGNS SHALL BE ORIENTATED AS SHOWN ON THE PLANS.
- 2) SIGN COLOR COMBINATIONS SHALL BE IN ACCORDANCE WITH THE FHWA SHS BOOK AND THE 2011 VIRGINIA SHS BOOK OR AS NOTED IN THE PLANS.
- 3) ALL POSITIVE CONTRAST GUIDE AND SPECIFIC SERVICE SIGNS SHALL UTILIZE FABRICATION LETTER TYPE L-3 OR L-4 UNLESS OTHERWISE NOTED IN THE REMARKS. ALL OTHER SIGNS SHALL UTILIZE FABRICATION LETTER TYPE L-1 OR L-2 UNLESS OTHERWISE NOTED IN THE REMARKS.
- 4) ALL BLACK SHEETING SHALL BE NON-REFLECTIVE.
- 5) SIGN STRUCTURES SHALL BE INSTALLED PER THE NOTED SIGN ST'D.
- 6) ALL ST'D. STP-1 STRUCTURES TO BE SINGLE POST UNLESS OTHERWISE NOTED.
- 7) IF APPLICABLE, SEE SHEET 2D FOR NON-STANDARD TYPE VA AND VIA SIGN STRUCTURE DETAILS.

SUBMITTED BY:

 FORT MYER CONSTRUCTION
 Since 1972

IN ASSOCIATION WITH:

 WBCM
 ARCHITECTURE ENGINEERING CONSTRUCTION

RFP CONCEPTUAL PLANS

THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.

TRAFFIC CONTROL DEVICE PLANS

SIGNING AND PAVEMENT MARKINGS SIGN SCHEDULE

PROJECT MANAGER: Amir, Salahsoor (703) 259-1957
 SURVEYED BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012
 DESIGN BY: J. Jaramas Group (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	620	0620-029-017, P-101	8(3)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

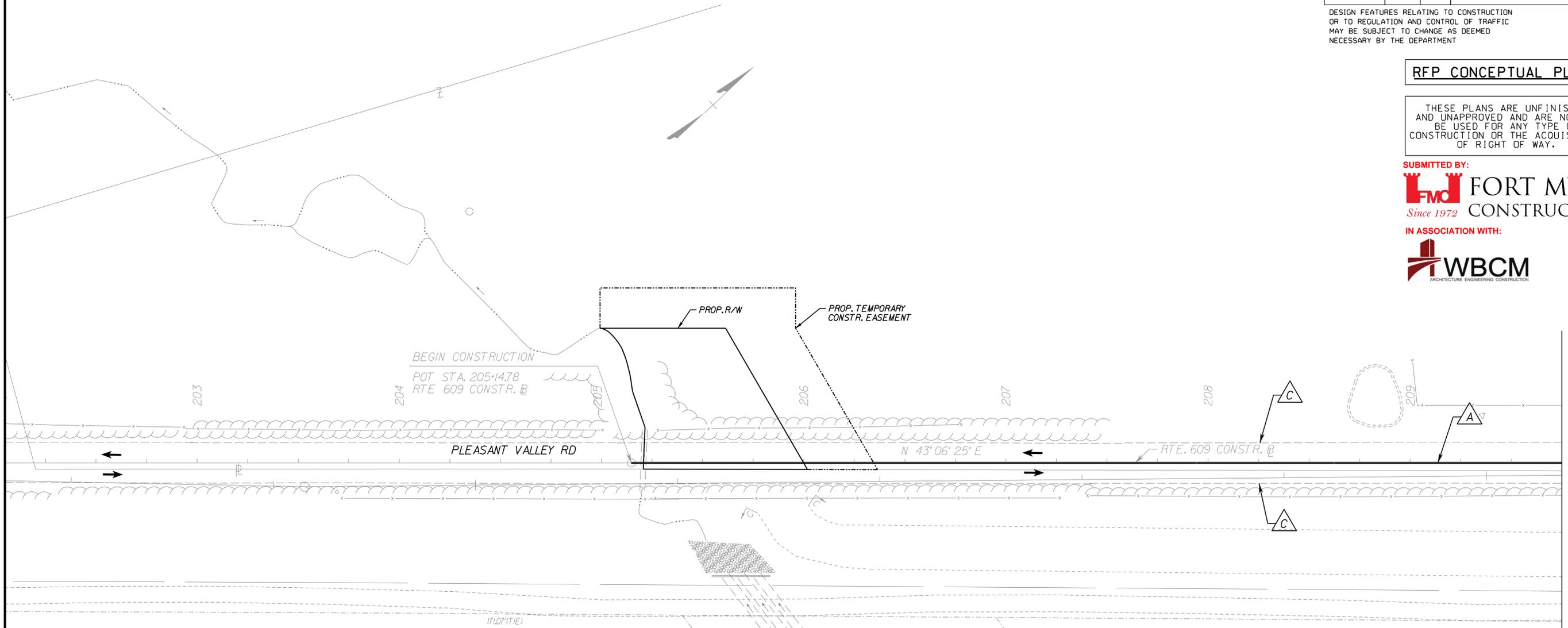
RFP CONCEPTUAL PLANS

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SUBMITTED BY:

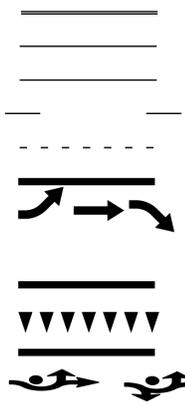


IN ASSOCIATION WITH:

PAVEMENT MARKING LEGEND

-  TYPE B, CLASS I DOUBLE YELLOW PAVEMENT LINE MARKING, 4" WIDTH
-  TYPE B, CLASS I YELLOW PAVEMENT LINE MARKING, 4" WIDTH
-  TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 4" WIDTH
-  TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 4" WIDTH (10' DASHED, 30' SPACING)
-  TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 4" WIDTH (2' DASHED, 4' SPACING)
-  TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 24" WIDTH
-  PAVEMENT MARKING ARROW (THRU ARROWS NOT TO BE PAINTED)
-  ERADICATE EXIST. PAVEMENT MARKINGS
-  TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 24" WIDTH (20' SPACING)
-  WHITE YIELD LINES (24" BASE WIDTH/36" LENGTH SPACED 3" TO 12")
-  TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 24" WIDTH (2' SPACING)
-  ROUNDABOUT PAVEMENT MARKING ARROW (FISH-HOOK)



MATCHLINE SHEET 8(4) - STA. 209+75.00

ROADWAY SHEET 5

PROJECT	0620-029-017	SHEET NO.	8(3)
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PROJECT MANAGER: Amr. Salahsoor (703) 259-1957
 SURVEYED BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012
 DESIGN BY: TJ Woods, Group (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	620	0620-029-017, P-101	

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

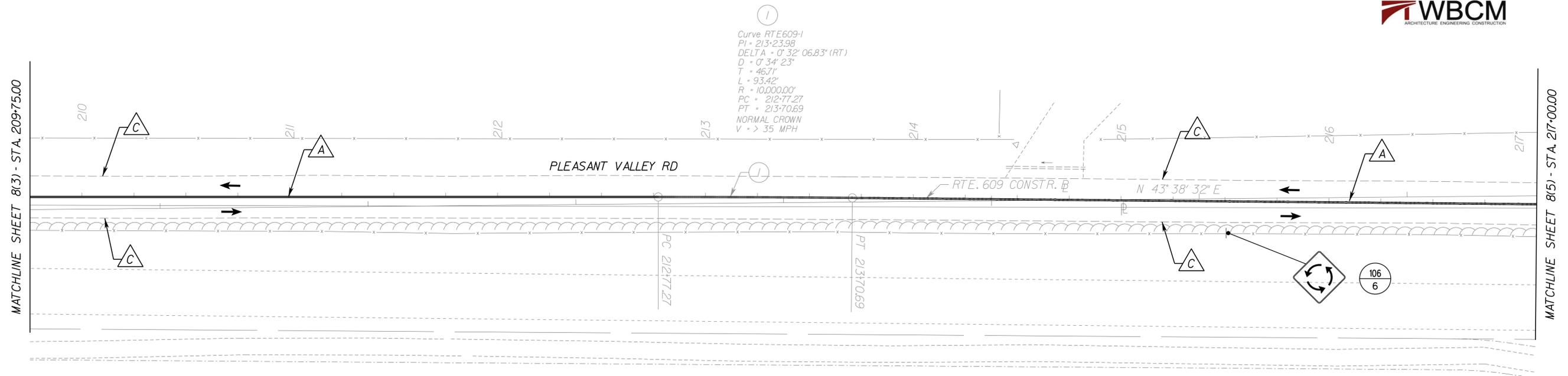
RFP CONCEPTUAL PLANS

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SUBMITTED BY:



IN ASSOCIATION WITH:



PAVEMENT MARKING LEGEND

- TYPE B, CLASS I DOUBLE YELLOW PAVEMENT LINE MARKING, 4" WIDTH
- TYPE B, CLASS I YELLOW PAVEMENT LINE MARKING, 4" WIDTH
- TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 4" WIDTH
- TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 4" WIDTH (10' DASHED, 30' SPACING)
- TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 4" WIDTH (2' DASHED, 4' SPACING)
- PAVEMENT MARKING ARROW (THRU ARROWS NOT TO BE PAINTED)
- ERADICATE EXIST. PAVEMENT MARKINGS
- TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 24" WIDTH (20' SPACING)
- WHITE YIELD LINES (24" BASE WIDTH/36" LENGTH SPACED 3" TO 12")
- TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 24" WIDTH (2' SPACING)
- ROUNDABOUT PAVEMENT MARKING ARROW (FISH-HOOK)



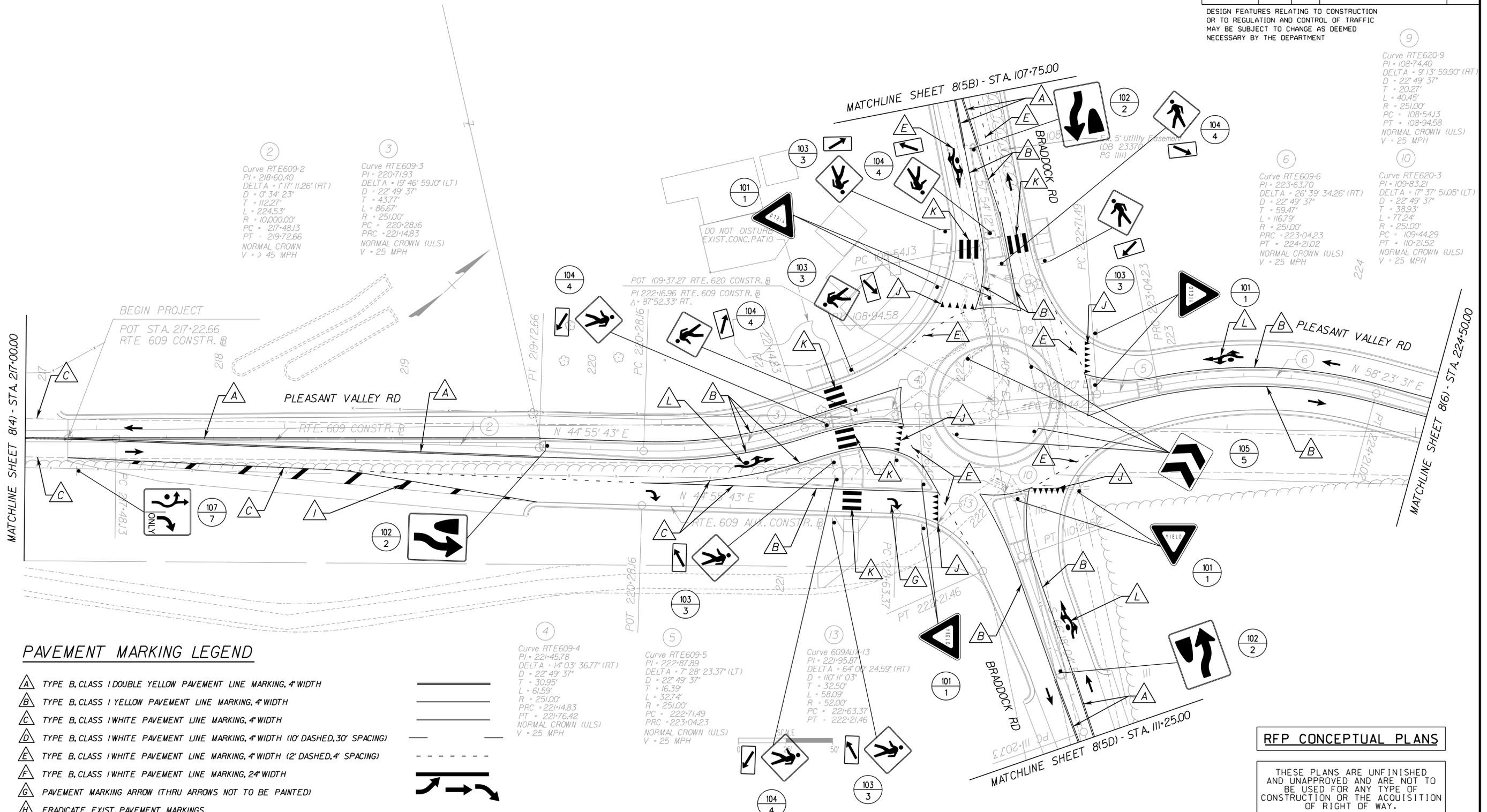
ROADWAY SHEET 8

PROJECT	SHEET NO.
0620-029-017	8(4)

PROJECT MANAGER: Amir Salahsoor (703) 259-1957
 SURVEYED BY, DATE: Leon Treutle, L.S. (V.DOT) 12/03/2012
 DESIGN BY: J. Tompkins Group (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leon Treutle, L.S. (V.DOT) 12/03/2012

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	620	0620-029-017, P-101	8(5)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



Curve RTE609-2
 PI = 218+60.40
 DELTA = 171° 11.26' (RT)
 D = 0° 34' 23"
 T = 112.27'
 L = 224.53'
 R = 10,000.00'
 PC = 217+48.13
 PT = 219+72.66
 NORMAL CROWN (ULS)
 V = > 45 MPH

Curve RTE609-3
 PI = 220+71.93
 DELTA = 19° 46' 59.0" (LT)
 D = 22° 49' 37"
 T = 43.77'
 L = 86.67'
 R = 251.00'
 PC = 220+28.16
 PRC = 221+4.83
 NORMAL CROWN (ULS)
 V = 25 MPH

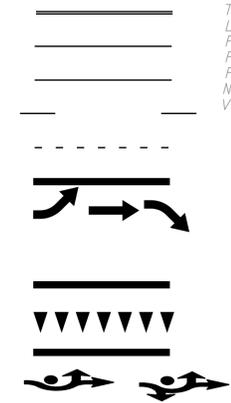
Curve RTE620-9
 PI = 108+74.40
 DELTA = 9° 13' 59.90" (RT)
 D = 22° 49' 37"
 T = 20.27'
 L = 40.45'
 R = 251.00'
 PC = 108+54.13
 PT = 108+94.58
 NORMAL CROWN (ULS)
 V = 25 MPH

Curve RTE609-6
 PI = 223+63.70
 DELTA = 26° 39' 34.26" (RT)
 D = 22° 49' 37"
 T = 59.47'
 L = 116.79'
 R = 251.00'
 PRC = 223+04.23
 PT = 224+21.02
 NORMAL CROWN (ULS)
 V = 25 MPH

Curve RTE620-3
 PI = 109+83.21
 DELTA = 17° 37' 51.05" (LT)
 D = 22° 49' 37"
 T = 38.93'
 L = 77.24'
 R = 251.00'
 PC = 109+44.29
 PT = 110+21.52
 NORMAL CROWN (ULS)
 V = 25 MPH

PAVEMENT MARKING LEGEND

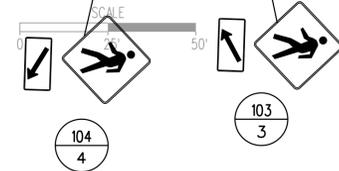
- A TYPE B, CLASS I DOUBLE YELLOW PAVEMENT LINE MARKING, 4" WIDTH
- B TYPE B, CLASS I YELLOW PAVEMENT LINE MARKING, 4" WIDTH
- C TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 4" WIDTH
- D TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 4" WIDTH (10' DASHED, 30' SPACING)
- E TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 4" WIDTH (2' DASHED, 4' SPACING)
- F TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 24" WIDTH
- G PAVEMENT MARKING ARROW (THRU ARROWS NOT TO BE PAINTED)
- H ERADICATE EXIST. PAVEMENT MARKINGS
- I TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 24" WIDTH (20' SPACING)
- J WHITE YIELD LINES (24" BASE WIDTH/36" LENGTH SPACED 3" TO 12')
- K TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 24" WIDTH (2' SPACING)
- L ROUNDABOUT PAVEMENT MARKING ARROW (FISH-HOOK)



Curve RTE609-4
 PI = 221+45.78
 DELTA = 14° 03' 36.77" (RT)
 D = 22° 49' 37"
 T = 30.95'
 L = 61.59'
 R = 251.00'
 PRC = 221+4.83
 PT = 221+6.42
 NORMAL CROWN (ULS)
 V = 25 MPH

Curve RTE609-5
 PI = 222+87.89
 DELTA = 7° 28' 23.37" (LT)
 D = 22° 49' 37"
 T = 16.39'
 L = 32.74'
 R = 251.00'
 PC = 222+71.49
 PRC = 223+04.23
 NORMAL CROWN (ULS)
 V = 25 MPH

Curve 609AUX-13
 PI = 221+95.87
 DELTA = 64° 00' 24.59" (RT)
 D = 110° 11' 03"
 T = 32.50'
 L = 58.09'
 R = 52.00'
 PC = 221+63.37
 PT = 222+21.46



RFP CONCEPTUAL PLANS

THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.

SUBMITTED BY:
FMD FORT MYER CONSTRUCTION
 Since 1972

IN ASSOCIATION WITH:
WBCM
 ARCHITECTURE ENGINEERING CONSTRUCTION

PROJECT	SHEET NO.
0620-029-017	8(5)

PROJECT MANAGER: Amr. Salahsoor (703) 259-1957
 SURVEYED BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012
 DESIGN BY: TJ Tompkins Group (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	620	0620-029-017, P-101	8(5B)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

RFP CONCEPTUAL PLANS

THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.

SUBMITTED BY:



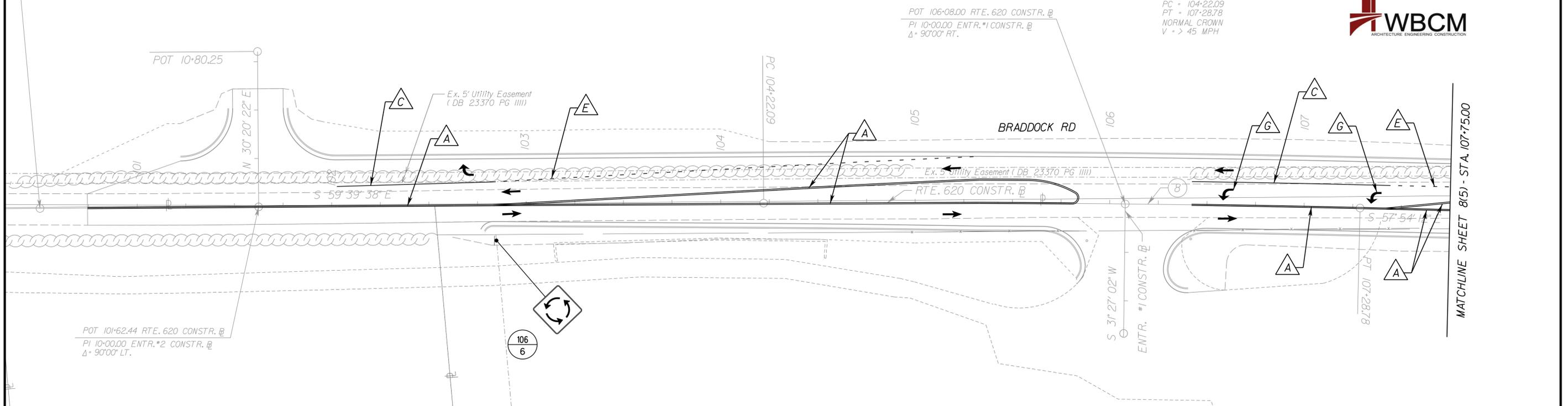
IN ASSOCIATION WITH:



BEGIN PROJECT 0620-029-017, P-101

STA. 100+50.00 ROUTE 620 CONSTR. \square
 END CONSTRUCTION
 POT STA. 100+50.00 ROUTE 620 CONSTR. \square

8
 Curve RTE620-8
 PI = 105+75.45
 DELTA = 1° 45' 25.88" (RT)
 D = 0° 34' 23"
 T = 153.36'
 L = 306.69'
 R = 10,000.00'
 PC = 104+22.09
 PT = 107+28.78
 NORMAL CROWN
 V = > 45 MPH



PAVEMENT MARKING LEGEND

- $\triangle A$ TYPE B, CLASS I DOUBLE YELLOW PAVEMENT LINE MARKING, 4" WIDTH
- $\triangle B$ TYPE B, CLASS I YELLOW PAVEMENT LINE MARKING, 4" WIDTH
- $\triangle C$ TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 4" WIDTH
- $\triangle D$ TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 4" WIDTH (10' DASHED, 30' SPACING)
- $\triangle E$ TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 4" WIDTH (2' DASHED, 4' SPACING)
- $\triangle F$ TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 24" WIDTH
- $\triangle G$ PAVEMENT MARKING ARROW (THRU ARROWS NOT TO BE PAINTED)
- $\triangle H$ ERADICATE EXIST. PAVEMENT MARKINGS
- $\triangle I$ TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 24" WIDTH (20' SPACING)
- $\triangle J$ WHITE YIELD LINES (24" BASE WIDTH/36" LENGTH SPACED 3" TO 12")
- $\triangle K$ TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 24" WIDTH (2' SPACING)
- $\triangle L$ ROUNDABOUT PAVEMENT MARKING ARROW (FISH-HOOK)



ROADWAY SHEET 5(B)

PROJECT	SHEET NO.
0620-029-017	8(5B)

PROJECT MANAGER: Amr. Salahsoor (703) 259-1957
 SURVEYED BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012
 DESIGN BY: TJ Woods Group (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012

REVISED	STATE	ROUTE	STATE PROJECT	SHEET NO.
	VA.	620	0620-029-017, P-101	8(5D)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

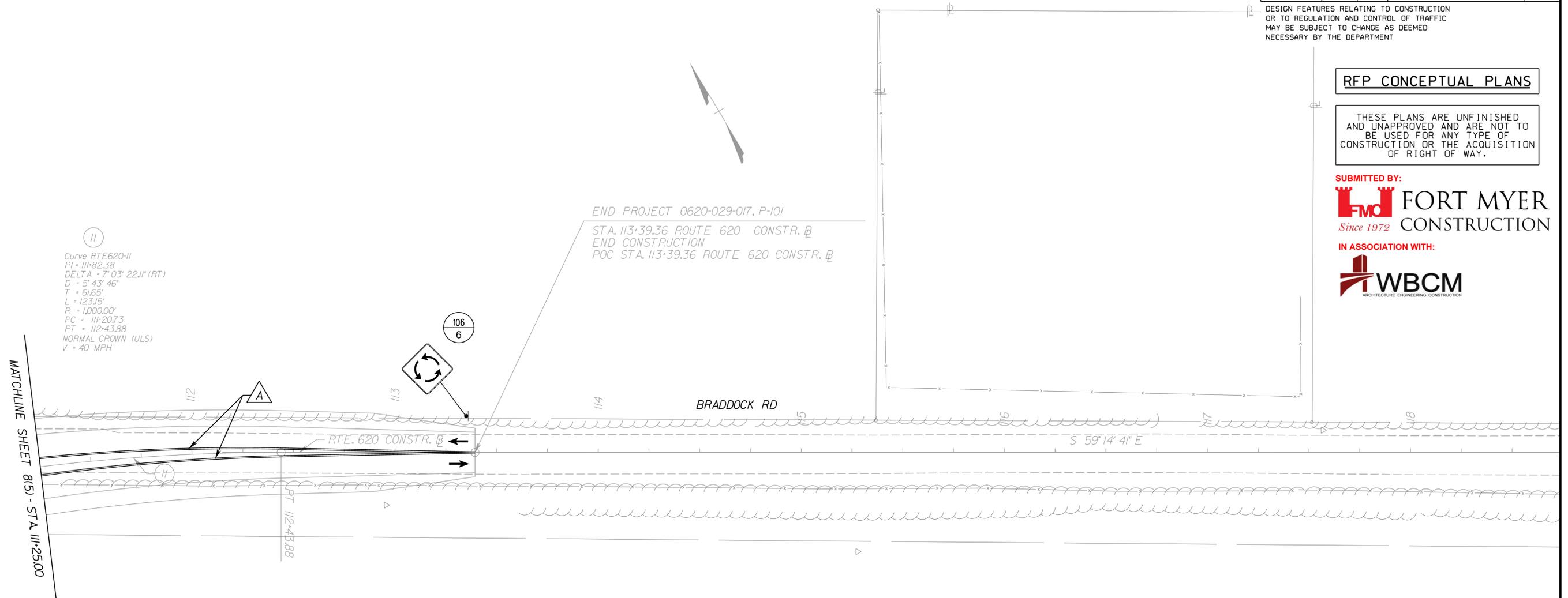
RFP CONCEPTUAL PLANS

THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.

SUBMITTED BY:



IN ASSOCIATION WITH:



PAVEMENT MARKING LEGEND

- A TYPE B, CLASS 1 DOUBLE YELLOW PAVEMENT LINE MARKING, 4" WIDTH
- B TYPE B, CLASS 1 YELLOW PAVEMENT LINE MARKING, 4" WIDTH
- C TYPE B, CLASS 1 WHITE PAVEMENT LINE MARKING, 4" WIDTH
- D TYPE B, CLASS 1 WHITE PAVEMENT LINE MARKING, 4" WIDTH (10' DASHED, 30' SPACING)
- E TYPE B, CLASS 1 WHITE PAVEMENT LINE MARKING, 4" WIDTH (2' DASHED, 4' SPACING)
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- K TYPE B, CLASS 1 WHITE PAVEMENT LINE MARKING, 24" WIDTH (2' SPACING)
- L ROUNDABOUT PAVEMENT MARKING ARROW (FISH-HOOK)



ROADWAY SHEET 5(D)

PROJECT	SHEET NO.
0620-029-017	8(5D)

PROJECT MANAGER: Amr. Salahsoor (703) 259-1957
 SURVEYED BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012
 DESIGN BY: TJ Woods, Group (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012

REVISED	STATE	STATE		SHEET NO.
	ROUTE	PROJECT		
	VA.	620	0620-029-017, P-101	8(6)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

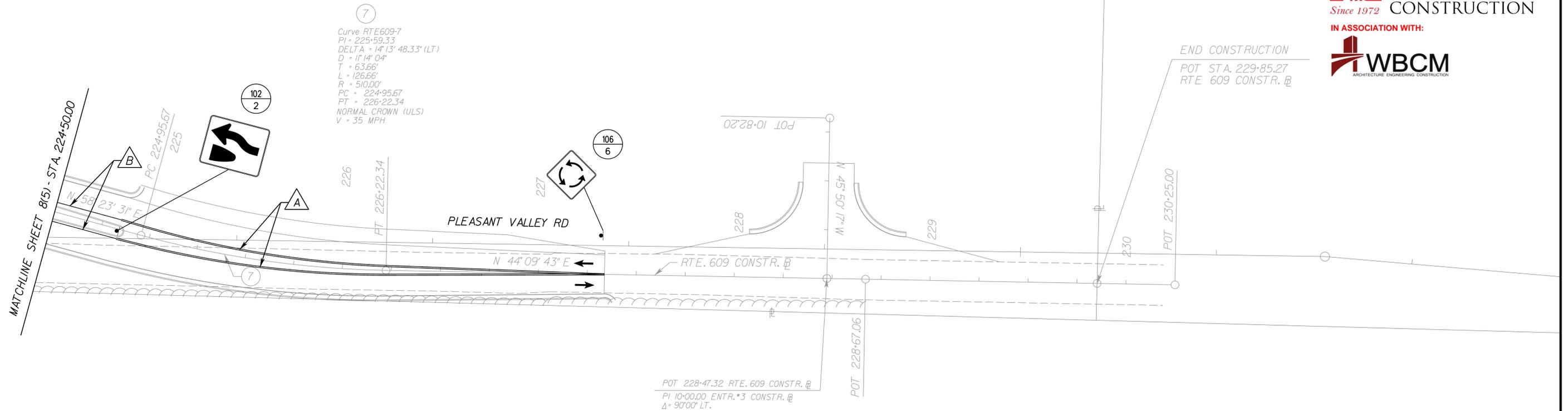
RFP CONCEPTUAL PLANS

THESE PLANS ARE UNFINISHED AND UNAPPROVED AND ARE NOT TO BE USED FOR ANY TYPE OF CONSTRUCTION OR THE ACQUISITION OF RIGHT OF WAY.

SUBMITTED BY:



IN ASSOCIATION WITH:



PAVEMENT MARKING LEGEND

- A TYPE B, CLASS I DOUBLE YELLOW PAVEMENT LINE MARKING, 4" WIDTH
- B TYPE B, CLASS I YELLOW PAVEMENT LINE MARKING, 4" WIDTH
- C TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 4" WIDTH
- D TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 4" WIDTH (10' DASHED, 30' SPACING)
- E TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 4" WIDTH (2' DASHED, 4' SPACING)
- F TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 24" WIDTH
- G PAVEMENT MARKING ARROW (THRU ARROWS NOT TO BE PAINTED)
- H ERADICATE EXIST. PAVEMENT MARKINGS
- I TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 24" WIDTH (20' SPACING)
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- K TYPE B, CLASS I WHITE PAVEMENT LINE MARKING, 24" WIDTH (2' SPACING)
- L ROUNDABOUT PAVEMENT MARKING ARROW (FISH-HOOK)



ROADWAY SHEET 6

PROJECT	SHEET NO.
0620-029-017	8(6)

PROJECT MANAGER: Amr. Salahsoo (703) 259-1957
 SURVEYED BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012
 DESIGN BY: TJ Woods Group (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012

REVISED	STATE	STATE		SHEET NO.
		ROUTE	PROJECT	
	VA.	620	0620-029-017, P-10f	9(2)

LANDSCAPE DETAIL SHEET

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT

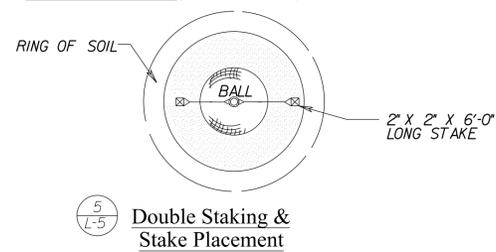
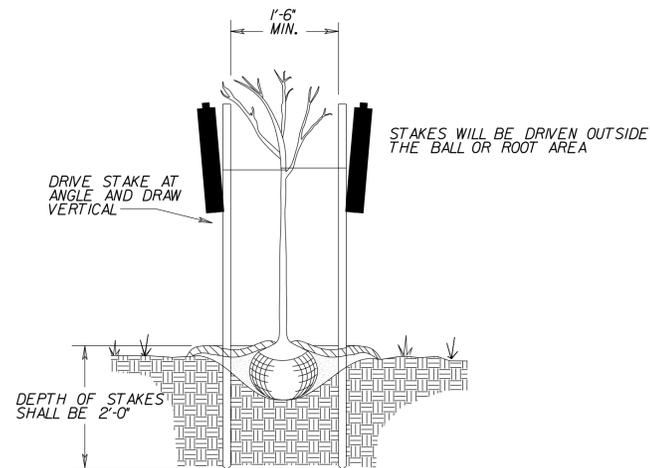
RFP CONCEPTUAL PLANS

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SUBMITTED BY:



IN ASSOCIATION WITH:



Notes:

1. ALL DECIDUOUS TREES OVER 8 FT. IN HEIGHT AND ALL EVERGREEN TREES OVER 8 FT. IN HEIGHT OR TALLER SHALL BE STAKED AND GUYED WITH 2 STAKES AS SHOWN.
2. MULTIPLE STEMMED DECIDUOUS TREES 8 FT. IN HEIGHT SHALL BE STAKED WITH 2 STAKES IN SUCH A MANNER AS TO STABILIZE 2 MAINSTEMS.
3. THE WOOD STAKES SHALL BE 2" X 2" X 6'-0" LONG DRESSED HARDWOOD AND DECAY RESISTANT.

THE WIRE TIES SHALL BE 14 GAUGE GALVANIZED WIRE AND BE PROVIDED WITH A ONE FOOT PIECE OF GREEN RUBBER HOSE PLACED TO PREVENT INJURY TO THE BARK. THERE SHOULD BE A 1" - 3" SWAY IN THE TREE (THE WIRES SHOULD NOT BE PULLED TIGHT) FOR BEST ESTABLISHMENT. OTHER ANCHORING METHODS AND MATERIALS MAY BE APPROVED FOR USE BY THE ENGINEER. NYLON TREE SUPPORT WEBBING MAY BE SUBSTITUTED FOR WIRE TIES.

**REMOVAL OF BINDING MATERIALS FROM TRUNKS AND STEMS, AND METAL CAGES FROM ROOT BALLS SHALL CONFORM TO THE CURRENT ROAD & BRIDGE SPECS.

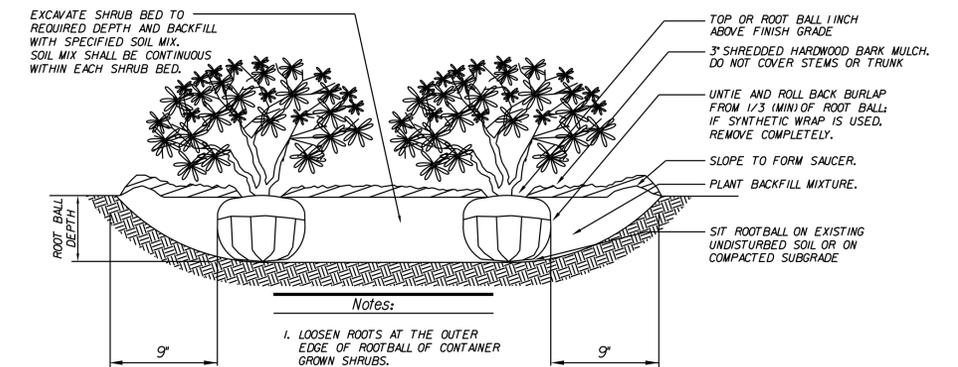
Planting Details

N.T.S.

Source: VDOT

10/02

1201_05



Notes:

1. LOOSEN ROOTS AT THE OUTER EDGE OF ROOTBALL OF CONTAINER GROWN SHRUBS.

Shrub Bed Planting

N.T.S.

Source: VHB

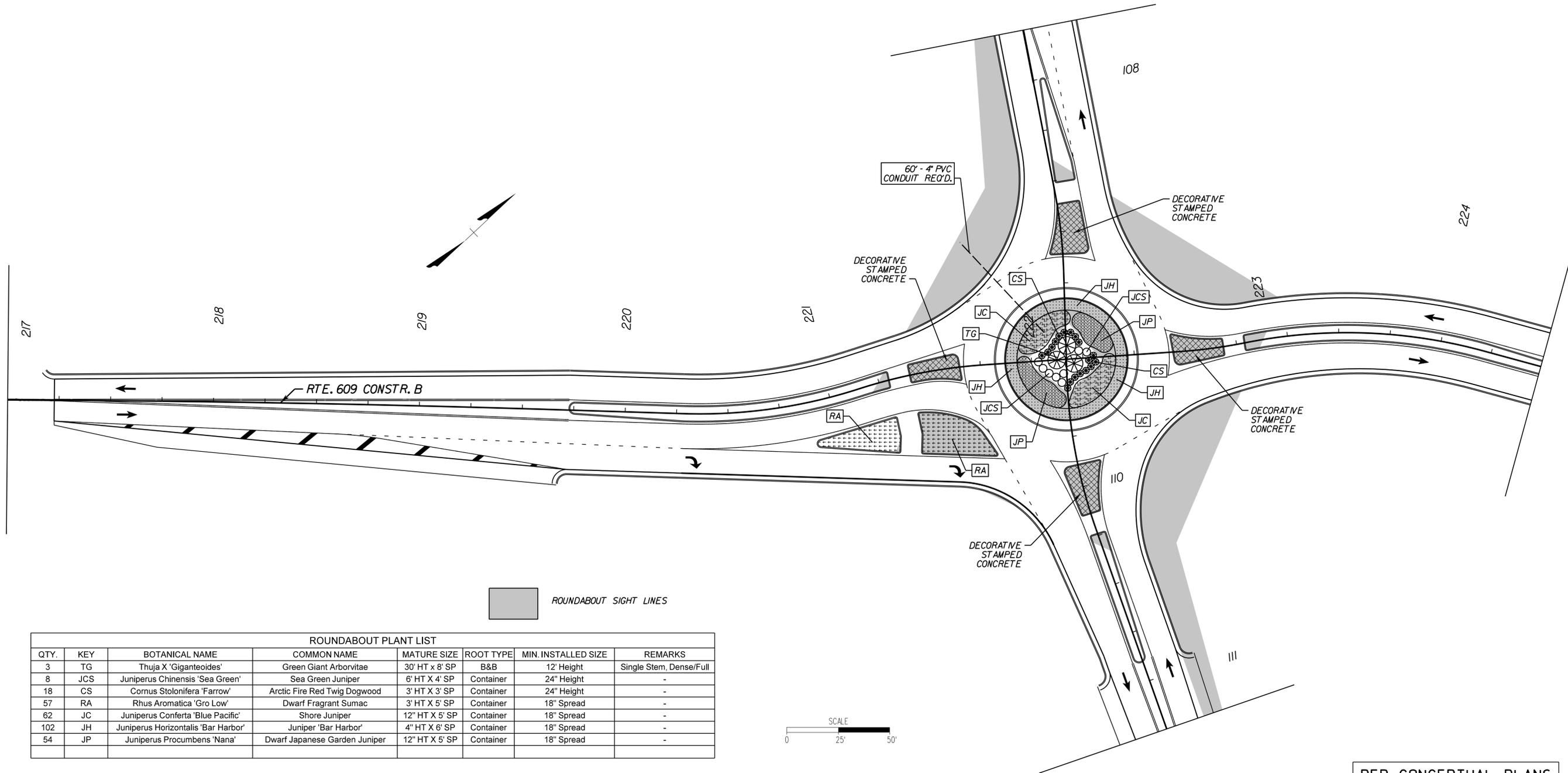
6/03

LD_602

PROJECT MANAGER: Amr. Salahsoor (703) 259-1957
 SURVEYED BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012
 DESIGN BY: TJ Woods, Group (804) 200-6500
 SUBSURFACE UTILITY BY, DATE: Leon Treutle, L.S. (VDOT) 12/03/2012

REVISED	STATE	STATE		SHEET NO.
	ROUTE	PROJECT		
	VA.	620	0620-029-017, P-101	9(3)

DESIGN FEATURES RELATING TO CONSTRUCTION OR TO REGULATION AND CONTROL OF TRAFFIC MAY BE SUBJECT TO CHANGE AS DEEMED NECESSARY BY THE DEPARTMENT



ROUNDABOUT PLANT LIST

QTY.	KEY	BOTANICAL NAME	COMMON NAME	MATURE SIZE	ROOT TYPE	MIN. INSTALLED SIZE	REMARKS
3	TG	Thuja X 'Giganteoides'	Green Giant Arborvitae	30' HT x 8' SP	B&B	12' Height	Single Stem, Dense/Full
8	JCS	Juniperus Chinensis 'Sea Green'	Sea Green Juniper	6' HT X 4' SP	Container	24" Height	-
18	CS	Cornus Stolonifera 'Farrow'	Arctic Fire Red Twig Dogwood	3' HT X 3' SP	Container	24" Height	-
57	RA	Rhus Aromatica 'Gro Low'	Dwarf Fragrant Sumac	3' HT X 5' SP	Container	18" Spread	-
62	JC	Juniperus Conferta 'Blue Pacific'	Shore Juniper	12" HT X 5' SP	Container	18" Spread	-
102	JH	Juniperus Horizontalis 'Bar Harbor'	Juniper 'Bar Harbor'	4" HT X 6' SP	Container	18" Spread	-
54	JP	Juniperus Procumbens 'Nana'	Dwarf Japanese Garden Juniper	12" HT X 5' SP	Container	18" Spread	-

RFP CONCEPTUAL PLANS

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COMPLIANCE OF TECHNICAL PROPOSAL

SECTION 4.2.8



SECTION 4.2.8 – FULL COMPLIANCE: In response to your Request for Proposals, Part 1, 4.2.8, Fort Myer Construction Corporation will demonstrate that our Conceptual Roadway Plans are fully compliant and meet or exceed the requirements of the Design Criteria included in the RFP Technical Requirements (Part 2) as well as all other requirements of the RFP. Additionally, located within the existing/proposed right-of-way limits are our proposed limits of construction that will include any stormwater management facilities. Our design concept does not require Design Exceptions and/or Design Waivers unless they are identified or included in the RFP or Addendum.

CD-ROM



DESIGN-BUILD PROJECT

Technical Proposal
April 24, 2014

**Braddock Road (Route 620) and Pleasant Valley Road (Route 609) Intersection Improvement
in Fairfax County, VA**



Submitted By:



**FORT MYER
CONSTRUCTION**

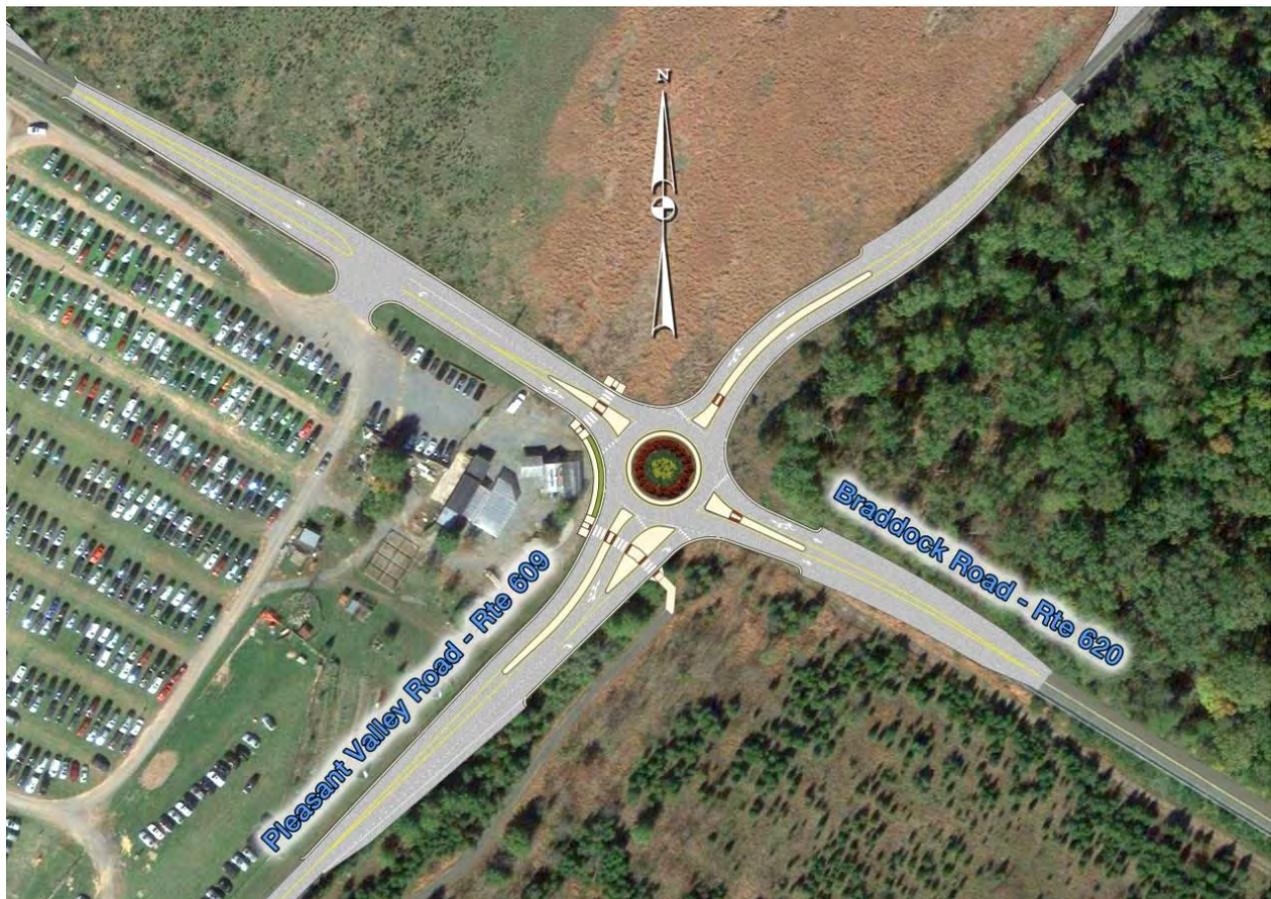
In Association With:



DESIGN-BUILD PROJECT

Braddock Road (Route 620) and Pleasant Valley Road (Route 609) Intersection Improvement in Fairfax County, VA

State Project No.: (NFO) 0620-029-017, P101, R201, C501
Federal Project No.: STP-5A01 (382)
Contract ID No.: C00103318DB71



Submitted By:



Since 1972

**FORT MYER
CONSTRUCTION**

In Association With:



PRICE PROPOSAL CHECKLIST

ATTACHMENT 4.0.1.2

ATTACHMENT 4.0.1.2

**DESIGN-BUILD PRICE PROPOSAL
CHECKLIST**

**Project Name: Braddock Road (Route 620) and Pleasant Valley Road
(Route 609) Intersection Improvement in Fairfax County, Virginia
Contract ID Number: C00103318DB71**

➤ Contents of Price Proposal:

- Proposal Price, in both numbers and words (Attachment 4.3.1)
 - ~~Schedule of Items itemized in accordance with Part 1, Section 4.4.6, including material quantities and costs of each proposed work package Addendum #1- April 8, 2014~~
 - ~~Proposed Monthly Payment Schedule showing the anticipated schedule on which funds will be required and associated value of work in accordance with Part 1, Section 4.4.7 Addendum #1- April 8, 2014~~
 - Price Adjustment Information and Forms for Fuel, Asphalt and Steel, including identification of pay items and associated quantities eligible for adjustment (Part 3, Section 6.3, Attachments 6.3)
 - Proposal Guaranty (C-24) required by Section 102.07 of Part 5, Division I Amendments to the Standard Specifications
 - Sworn Statement Forms (C-104, C-105, Attachments 4.3.4(a) and 4.3.4(b))
 - CD-ROM containing the entire Price Proposal in a single cohesive Adobe PDF file
-

COST BREAKDOWN SUMMARY

ATTACHMENT 4.3.1

ATTACHMENT 4.3.1

PRICE PROPOSAL FORM

4.3.1 Offeror shall specify the pricing information for the items below, the dollars amount shall be in whole numbers:

Price Proposal Cost Breakdown Summary;

Design Services, LS	<u>\$ 758,000.00</u>
Mobilization (Construction), LS	<u>\$ 325,000.00</u>
Quality Assurance (QA) (Construction), LS	<u>\$ 105,000.00</u>
Quality Control (QC) (Construction), LS	<u>\$ 105,000.00</u>
Earthwork, LS	<u>\$ 200,000.00</u>
Roadway Incidentals, LS	<u>\$ 695,333.00</u>
Drainage (Structures), LS	<u>\$ 180,000.00</u>
Utilities, LS	<u>\$ 250,000.00</u>
Pavement, LS	<u>\$ 425,000.00</u>
Permanent Traffic Control/Signage, LS	<u>\$ 60,000.00</u>
Maintenance of Traffic, LS	<u>\$ 250,000.00</u>
All Others Costs, LS	<u>\$ 825,000.00</u>

Proposal Price; (Specify the Total Lump Sum price in both numbers and words, this price shall **equal** to the total sum of the items listed above)

Lump Sum (LS): Four Million One Hundred Seventy-Eight Thousand
Three Hundred Thirty-Three and no cents (\$4,178,333.00)

Signature:  Date: April 24, 2014

Jose Rodriguez, President

Design-Builder: Fort Myer Construction Corporation

Vendor No.: F034



Braddock Road (Route 620) and Pleasant Valley Road (Route 609)
Intersection Improvement in Fairfax County, VA
State Project No.: (NFO) 0620-029-017, P101, R201, C501
Federal Project No.: STP-5A01 (382); Contract ID No. C00103318DB71

PRICE ADJUSTMENT INFORMATION, AND FORMS FOR FUEL, ASPHALT AND STEEL

SECTION 6.3



**EXHIBIT 6.3 (c)
ADJUSTMENT FOR FUEL**

**VIRGINIA DEPARTMENT OF TRANSPORTATION
SPECIAL PROVISION FOR
OPTIONAL ADJUSTMENT FOR FUEL
DESIGN-BUILD PROJECTS**

November 5, 2012

In the event the Design-Builder elects to seek adjustment for fuel items designated in the Price Proposal/Contract as Price Adjustment Items such items will be subject to price adjustment as set forth herein. Other items will not be adjusted, except as otherwise specified in the contract.

The Design-Builder will submit their monthly application for payment associated with eligible work packages with an adjustment up or down as appropriate for cost changes in fuel used on specific items of work identified in this provision. A master listing of standard items eligible for fuel adjustment is provided by the Department on its website at the following link <http://www.virginiadot.org/business/resources/masteroptionalfuelitems.pdf>. The listing on the web site also includes the corresponding fuel factor for each item. The fuel usage factor for each item is considered inclusive of all fuel usage.

The amount of adjustment will be computed from the change in the indexes and the on-site fuel use as shown in the Department's master listing of eligible items.

In order to be eligible for fuel adjustment under this provision, the Design-Builder shall clearly identify in within the Schedule of Values those pay items and the associated quantities it chooses to have fuel adjustment applied to in its work packages. Items the Design-Builder claims in its application of payment for fuel adjustments must be properly designated in order to be considered for adjustment. Items not properly designated or left out of the Design-Builder's Schedule of Values will automatically not be considered for adjustment.

The monthly index price to be used in the administration of this provision will be calculated by the Department from the Diesel fuel prices published by the U. S. Department of Energy, Energy Information Administration on highway diesel prices, for the Lower Atlantic region. The monthly index price will be the price for diesel fuel calculated by averaging each of the weekly posted prices for that particular month.

For the purposes of this provision, the base index price will be calculated using the data from the month preceding the receipt of bids. The base index price will be posted by the Department at the beginning of the month for all bids received during that month.

The current index price will be posted by the Department and will be calculated using the data from the month preceding the particular estimate being vouchered for payment.

The current monthly quantity for eligible items of work selected by the Design-Builder for fuel adjustment in its work packages will be multiplied by the appropriate fuel factor to determine the gallons of fuel to be cost adjusted. The amount of adjustment per gallon will be the net difference between the current index price and the base index price. Computation for adjustment will be made as follows:

$$S = (E - B) QF$$

Where; S = Monetary amount of the adjustment (plus or minus)
B = Base index price
E = Current index price
Q = Quantity of individual units of work

F = Appropriate fuel factor

Adjustments will not be made for work performed beyond the original contract time limit unless the original time limit has been changed by an executed Work Order.

If new pay items are added to this contract by Work Order and they are listed in the Department's master listing of eligible items, the Work Order must indicate which of these individual items will be fuel adjusted; otherwise, those items will not be fuel adjusted. If applicable, designating which new pay items will be added for fuel adjustment must be determined during development of the Work Order and clearly shown on the Work Order form. The Base Index price on any new eligible pay items added by Work Order will be the Base Index price posted for the month in which bids were received for that particular project. The Current Index price for any new eligible pay items added by Work Order will be the Index price posted for the month preceding the estimate on which the Work Order is paid.

When quantities differ between the last monthly application of payment prepared upon final acceptance and the final application of payment, adjustment will be made using the appropriate current index for the period in which that specific item of work was last performed.

In the event any of the base fuel prices in this contract increase more than 100 percent (i.e. fuel prices double), the Department will review each affected item of work and give the Design-Builder written notice if work is to stop on any affected item of work. The Department reserves the right to reduce, eliminate or renegotiate the price for remaining portions of affected items of work.

Any amounts resulting from fuel adjustment will not be included in the total cost of work for determination of progress or for extension of contract time.

I elect to use this provision

I elect not to use this provision

Date: 04/23/2014

Signature: 

Design-builder: FORT MYER CONSTRUCTION CORP

Vendor No.: F034

EXHIBIT 6.3(d)
ADJUSTMENT FOR STEEL

VIRGINIA DEPARTMENT OF TRANSPORTATION
SPECIAL PROVISION FOR
PRICE ADJUSTMENT FOR STEEL
DESIGN-BUILD PROJECTS

June 30, 2011

In the event the Design-Builder elects to seek adjustment for steel items designated in the Price Proposal/Contract as Price Adjustment Items such items will be subject to price adjustment as set forth herein. If new pay items which involve steel are established by Work Order, they will not be subject to Price Adjustment unless specifically designated in the Work Order to be subject to Price Adjustment.

The Design-Builder will submit their monthly application for payment associated with eligible work packages with an adjustment up or down as appropriate for cost changes in steel used on specific items of work identified in the Price Proposal/contract in accordance with this provision. Provided at the end of this provision is a master listing of standard bid items the Department has determined are eligible for steel price adjustment. Inventoried materials from the listing of eligible items are specifically excluded for consideration. In addition, concrete items where reinforcing steel is normally included in the unit bid price for the item such as (but not limited to) drop inlets, median barriers, sound barrier walls, bridge railing and parapets, are not eligible for consideration under this provision.

The requirements of this provision shall apply only to material cost changes that occur between the date of the opening of the Price Proposal and the date the material is shipped to the fabricator. To be eligible for this price adjustment, Design-Builder is required to fill out the accompanying Form for Price Adjustment for Eligible Steel Items on Design-Build Projects and submit the same with its Price Proposal for the Project. By signing the Form and submitting it with its Price Proposal Design-Builder declares its intention to participate in the price adjustment in its contract with the Department. For the purposes of this provision, the prices listed on the Form for Price Adjustment for Eligible Steel Items on Design-Build projects are fixed for cost and adjustment calculations regardless of quantities incorporated into final design. Further, in order for steel items to be eligible for adjustment, once shipped to the fabricator, the items shall be specifically stored, labeled, or tagged, recognizable by color marking, and identifiable by project for inspection and audit verification.

Design-Builder shall upon request furnish documentation supporting the price per pound for eligible steel items as shown on the Form for Price Adjustment for Eligible Steel Items on Design-Build Projects furnished with its Price Proposal. Design-Builder must use the format as shown with this Form; no other format for presenting this information will be permitted. Design-Builder shall certify that all items of documentation are original and were used in the computation of the price per pound amount for the represented eligible pay items for the month the Price Proposal was opened. This documentation shall support the base line material price ("Base Price") of the steel item only. Base price per pound shall not include the following cost components: fabrication, shipping, storage, handling, and erection.

Failure to submit all documentation required or requested supporting the per pound prices on eligible steel items will result in Design-Builder being ineligible for a price adjustment of any or all steel items.

Price adjustment of each qualifying item under consideration will be subject to the following condition:

There is an increase or decrease in the cost of eligible steel materials in excess of 10 percent up to a maximum of 60 percent from the Base Price when compared with the latest published price index ("Price Index") in effect at the time material is shipped to the fabricator.

The Price Index the Department is using is based on The U.S. Department of Labor, Bureau of Labor Statistics, Producers Price Index (PPI) which measures the average price change over time of the specific

steel eligible item from the perspective of the seller of goods. The Master List table provided at the end of this provision indicates the Producers Price Index (PPI) steel category index items and the corresponding I.D. numbers to which VDOT items will be compared. **Please note:** The Producers Price Index (PPI) is subject to revision 4 months after original publication, therefore, price adjustments and payments will not be made until the index numbers are finalized.

The price adjustment will be determined by computing the percentage of change in index value beyond 10 percent above or below the index on the date of opening of Design-Builder's Price Proposal to the index value on the date the steel material is shipped to the fabricator (Please see included sample examples). Weights and date of shipment must be documented by a bill of lading provided to the Department. The final price adjustment dollar value will be determined by multiplying this percent increase or decrease in the index (after 10%) by the represented quantity of steel shipped, by the Base Price per pound subject to the limitations herein.

Price increase/decrease will be computed as follows:

$$A = B \times P \times Q$$

- Where;
- A = Steel price adjustment in lump sum dollars
 - B = Average weighted price of steel submitted in Design-Builder's Price Proposal for project in price per pound as listed on the Form for Price Adjustment for Eligible Steel Items on Design-Build Project
 - P = Adjusted percentage change in PPI average from shipping date to date of opening of Price Proposal minus 10% (0.10) threshold
 - Q = Total quantity of steel in pounds shipped to fabricator for specific project

The need for application of the adjustments herein to extra work will be determined by the Engineer on an individual basis and, if appropriate, will be specified on the Work Order.

This price adjustment is capped at 60 percent. This means the maximum "P" value for increase or decrease that can be used in the above equation is 50% (60%-10% threshold).

Calculations for price adjustment shall be shown separate from the monthly progress payment for work packages and will not be included in the total cost of work for determination of progress or for extension of contract time.

Upon Department review and due process consideration for redress by Design-Builder, any apparent evidence to unbalance the price supplied by Design-Builder in favor of items subject to price adjustment will result in ineligibility for Department participation under this provision.

Sample Calculation of a Price Adjustment (increase)

Project bid on April 28, 2004.

Project has 450,000 lb. of eligible structural steel.

Design Builder's *f.o.b. supplier price for structural steel submitted in the Price Proposal is \$0.2816 per pound. *free on board

Adjusted** BLS Producers Price Index (PPI) most recently published average at time of opening of the Price Proposal is 139.6.

All eligible steel shipped to fabricator in same month, October 2004.

Adjusted BLS Producers Price Index (PPI) most recently published average for month of October is 161.1

Adjustment formula is as follows:

$$A = B \times P \times Q$$

Where;

- A = Steel price adjustment in lump sum dollars
- B = Average weighted price of steel submitted in the Price Proposal for Design-Build project in \$ per pound
- P = Adjusted percentage change in PPI average from shipping date to date of submitted Price Proposal minus 10% (0.10) threshold
- Q = Total quantity of eligible steel shipped to fabricator in October 2004 for this project in pounds

$$B = \$0.2816$$

$$P = (161.1 - 139.6) / 139.6 - 0.10 = 0.054$$

$$Q = 450,000 \text{ lb.}$$

$$A = 0.2816 \times 0.054 \times 450,000$$

$$A = \$6,842.88 \text{ pay adjustment to Design-Builder}$$

Sample Calculation of a Price Adjustment (decrease)

Project bid on April 28, 2004.

Project has 450,000 lb. of eligible structural steel.

Design-Builder's *f.o.b. supplier price for structural steel submitted in the Price Proposal is \$0.2816 per pound. *free on board

Adjusted BLS Producers Price Index (PPI) most recently published average at time of opening of the Price Proposal is 156.6.

All eligible steel shipped to fabricator in same month, October 2004.

Adjusted BLS Producers Price Index (PPI) most recently published average for month of October is 136.3

Adjustment formula is as follows:

$$A = B \times P \times Q$$

Where;

- A = Steel price adjustment in lump sum dollars
- B = Average weighted price of steel submitted in the Price Proposal for Design-Build project in \$ per pound
- P = Adjusted percentage change in PPI average from shipping date to date of submitted Price Proposal minus 10% (0.10) threshold
- Q = Total quantity of eligible steel shipped to fabricator in October 2004 for this project in pounds

$$B = \$0.2816$$

$$P = (156.6 - 136.3) / 156.6 - 0.10 = 0.030$$

$$Q = 450,000 \text{ lb.}$$

$$A = 0.2816 \times 0.030 \times 450,000$$

$$A = \$3,801.60 \text{ credit to Department}$$

MASTER LISTING

STANDARD BID ITEMS ELIGIBLE FOR STEEL PRICE ADJUSTMENT

March 18, 2009

BLS Series I. D.

ITEM NUMBER	ITEM DESCRIPTION	UNITS	Number WPU used in \$ adjust.
00519	SHEET PILE, STEEL	SF	avg. 1017 & 101
00540	REINF. STEEL	LB	101704
00542	EPOXY COATED REINF. STEEL	LB	101704
00560	STRUCTURAL STEEL JB-1	LB	avg. 1017 & 101
11030	REINF. STEEL BRIDGE APPR. SLAB	LB	101704
11181	PATCH.HYDR.CEM.CONC. PAVE.	SY	101704
13290	GUARDRAIL GR-8 (NCHRP 350 TL-3)	LF	avg. 1017 & 101
13292	GUARDRAIL GR-8A (NCHRP 350 TL-3)	LF	avg. 1017 & 101
13294	GUARDRAIL GR-8B (NCHRP 350 TL-3)	LF	avg. 1017 & 101
13310	GUARDRAIL TERMINAL GR-6 (NCHRP 350)	LF	avg. 1017 & 101
13320	GUARDRAIL GR-2	LF	avg. 1017 & 101
13323	GUARDRAIL GR-2A	LF	avg. 1017 & 101
13331	RAD. GUARDRAIL GR-2	LF	avg. 1017 & 101
13333	RAD. GUARDRAIL GR-2A	LF	avg. 1017 & 101
13335	GUARDRAIL GR-3	LF	avg. 1017 & 101
13341	GUARDRAIL TER. GR-6(WEATHERING STEEL)	LF	avg. 1017 & 101
13351	GUARDRAIL GR-8	LF	avg. 1017 & 101
13352	GUARDRAIL GR-8A	LF	avg. 1017 & 101
13353	GUARDRAIL GR-8B	LF	avg. 1017 & 101
13355	GUARDRAIL GR-10	LF	avg. 1017 & 101
13421	MEDIAN BARRIER MB-3	LF	avg. 1017 & 101
13450	MEDIAN BARRIER MB-5	LF	avg. 1017 & 101
13451	MEDIAN BARRIER MB-5A	LF	avg. 1017 & 101
13452	MEDIAN BARRIER MB-5B	LF	avg. 1017 & 101
13545	REINF. STEEL	LB	101704
14502	REINFORCING STEEL	LB	101704
15290	PATCH.CEM.CONC.PAVE.TY.CRCP-A	SY	101704
15302	PATCH.CEM.CONC.PAVE. TY. II	SY	101704
15305	PATCH.CEM.CONC.PAVE.TY. IV-A	SY	101704
17323	GUARDRAIL BEAM *	LF	avg. 1017 & 101
17325	RADIAL GUARDRAIL BEAM *	LF	avg. 1017 & 101
17327	RUB RAIL	LF	avg. 1017 & 101
17353	CABLE GR-3	LF	avg. 1017 & 101
17521	GUARDRAIL BEAM (WEATHERING STEEL)	LF	avg. 1017 & 101
17523	RADIAL GUARDRAIL BEAM (WEATHERING STEEL)	LF	avg. 1017 & 101
17525	RUB RAIL (WEATHERING STEEL)	LF	avg. 1017 & 101
22501	FENCE FE-W1	LF	avg. 1017 & 101
22643	FENCE FE-CL	LF	avg. 1017 & 101
22645	FENCE FE-CL VINYL COATED	LF	avg. 1017 & 101
23043	WATER GATE FE-4 TY.III	LF	avg. 1017 & 101
23501	FENCE FE-W1 (FABRIC ONLY)	LF	avg. 1017 & 101
45522	4" STEEL ENCASE. PIPE	LF	101706
45532	6" STEEL ENCASE. PIPE	LF	101706
45562	16" STEEL ENCASE. PIPE	LF	101706
45572	18" STEEL ENCASE. PIPE	LF	101706

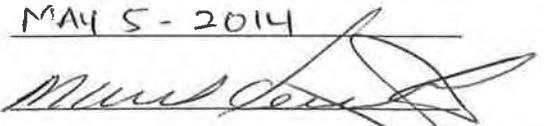
45582	24" STEEL ENCASE. PIPE	LF	101706
45584	24" JACKED STEEL ENCASUREMENT PIPE	LF	101706
45592	30" STEEL ENCASE. PIPE	LF	101706
50402	SIGN POST STEEL 3"	LF	101706
50404	SIGN POST STEEL 4"	LF	101706
50406	SIGN POST STEEL 6"	LF	101706
50410	SIGN POST STEEL 10"	LF	101706
50412	SIGN POST STEEL 12"	LF	101706
50414	SIGN POST STEEL 14"	LF	101706
50416	SIGN POST STEEL 16"	LF	101706
50418	SIGN POST STEEL 18"	LF	101706
51317	SIG. POLE MP-1 20' ONE ARM 30'	EA	101706
51319	SIG. POLE MP-1 20' ONE ARM 32'	EA	101706
51325	SIG. POLE MP-1 20' ONE ARM 38'	EA	101706
51327	SIG. POLE MP-1 20' ONE ARM 40'	EA	101706
51329	SIG. POLE MP-1 20' ONE ARM 42'	EA	101706
51331	SIG. POLE MP-1 20' ONE ARM 44'	EA	101706
51337	SIG. POLE MP-1 20' ONE ARM 50'	EA	101706
51339	SIG. POLE MP-1 20' ONE ARM 52'	EA	101706
51341	SIG. POLE MP-1 20' ONE ARM 54'	EA	101706
51344	SIG. POLE MP-1 20' ONE ARM 56'	EA	101706
51346	SIG. POLE MP-1 20' ONE ARM 58'	EA	101706
51347	SIG. POLE MP-1 20' ONE ARM 60'	EA	101706
51348	SIG. POLE MP-1 20' ONE ARM 62'	EA	101706
51368	SIG.POLE MP-1 20'TWO ARMS 36'& 42'	EA	101706
51400	SIG.POLE MP-1 CO.LU.ONE ARM 38	EA	101706
51402	SIG.POLE MP-1 CO.LU.ONE ARM 40	EA	101706
51408	SIG.POLE MP-1 CO.LU.ONE ARM 46	EA	101706
51412	SIG.POLE MP-1 CO.LU.ONE ARM 50	EA	101706
51414	SIG.POLE MP-1 CO.LU.ONE ARM 52	EA	101706
51416	SIG.POLE MP-1 CO.LU.ONE ARM 54	EA	101706
51418	SIG.POLE MP-1 CO.LU.ONE ARM 56	EA	101706
51420	SIG.POLE MP-1 CO.LU.ONE ARM 58	EA	101706
51422	SIG.POLE MP-1 CO.LU.ONE ARM 60	EA	101706
55162	LIGHTING POLE LP-1 30'-4'	EA	101706
55163	LIGHTING POLE LP-1 30'-6'	EA	101706
55166	LIGHTING POLE LP-1 30'-12'	EA	101706
55169	LIGHTING POLE LP-1 35'-6'	EA	101706
55171	LIGHTING POLE LP-1 35'-10'	EA	101706
55176	LIGHTING POLE LP-1 40'-8'	EA	101706
55185	LIGHTING POLE LP-2 TYPE A	EA	101706
55186	LIGHTING POLE LP-2 TYPE B	EA	101706
55187	LIGHTING POLE LP-2 TYPE C	EA	101706
55188	LIGHTING POLE LP-2 TYPE D	EA	101706
55189	LIGHTING POLE LP-2 TYPE E	EA	101706
55190	LIGHTING POLE LP-2 TYPE F	EA	101706
55192	LIGHTING POLE LP-2 TYPE H	EA	101706
60452	REINF. STEEL BRIDGE APPR. SLAB	LB	101704
61700	REINF. STEEL	LB	101704
61704	CORROSION RESISTANT REINF. STEEL	LB	101704
61705	EPOXY COATED REINF. STEEL	LB	101704
61750	STRUCT.STEEL HIGH STRG.PLT.GIRDERS	LB	avg. 1017 & 101
61811	STR.STEEL PLATE GIRDER ASTM A709 GRADE50	LB	avg. 1017 & 101
61812	STR.STEEL PLATE GIRDER ASTM A709 GRADE50	LB	avg. 1017 & 101
61813	STR.STEEL PLATE GIRDER ASTM A709 GRADEHPS50W	LB	avg. 1017 & 101
61814	STR.STEEL PLATE GIRDER ASTM A709 GRADEHPS70W	LB	avg. 1017 & 101
61820	STR.STEEL ROLLED BEAM ASTM A709 GRADE 36	LB	avg. 1017 & 101

61821	STR.STEEL ROLLED BEAM ASTM A709 GRADE50	LB	avg. 1017 & 101
61822	STR.STEEL ROLLED BEAM ASTM A709 GRADE50W	LB	avg. 1017 & 101
61990	STEEL GRID FLOOR	SF	avg. 1017 & 101
64110	STEEL PILES 10"	LF	avg. 1017 & 101
64112	STEEL PILES 12"	LF	avg. 1017 & 101
64114	STEEL PILES 14"	LF	avg. 1017 & 101
64768	DRIVING TEST FOR 12" STEEL PILE	LF	avg. 1017 & 101
64778	DRIVING TEST FOR 14" STEEL PILE	LF	avg. 1017 & 101
65200	REINF. STEEL	LB	101704
65204	CORROSION RESISTANT REINF. STEEL	LB	101704
65205	EPOXY COATED REINF. STEEL	LB	101704
67086	PED. FENCE 6'	LF	avg. 1017 & 101
67088	PED. FENCE 8'	LF	avg. 1017 & 101
67089	PED. FENCE 10'	LF	avg. 1017 & 101
68100	REINF. STEEL	LB	101704
68104	CORROSION RESISTANT REINF. STEEL	LB	101704
68105	EPOXY COATED REINF. STEEL	LB	101704
68107	STR.STEEL PLATE GIRDER ASTM A709 GRADE50	LB	avg. 1017 & 101
68108	STR. STEEL PLATE GIRDER ASTM A709 GR50W	LB	avg. 1017 & 101
68109	STR. STEEL PLATE GIRDER ASTM A709 GR.HPS50W	LB	avg. 1017 & 101
68110	STR. STEEL PLATE GIRDER ASTM A709 GR.HPS70W	LB	avg. 1017 & 101
68112	STR.STEEL ROLLED BEAM ASTM A709 GR.36	LB	avg. 1017 & 101
68113	STR.STEEL ROLLED BEAM ASTM A709 GR.50	LB	avg. 1017 & 101
68114	STR.STEEL ROLLED BEAM ASTM A709 GR. 50W	LB	avg. 1017 & 101
68115	STRUCT. STEEL	LB	avg. 1017 & 101
68270	REINF. STEEL BRIDGE APPR. SLAB	LB	101704
69060	SHEET PILES, STEEL	SF	avg. 1017 & 101
69100	REINF. STEEL	LB	101704
69104	CORROSION RESISTANT REINF. STEEL	LB	101704
69105	EPOXY COATED REINF. STEEL	LB	101704
69110	STEEL PILES 10"	LF	avg. 1017 & 101
69112	STEEL PILE 12"	LF	avg. 1017 & 101
69113	DRIVING TEST FOR 12" STEEL PILE	LF	avg. 1017 & 101

I elect to use this provision

I elect not to use this provision

Date: MAY 5 - 2014

Signature: 

Design-Builder: FORT MYER CONSTRUCTION

Vendor No.: F034



**Braddock Road (Route 620) and Pleasant Valley Road (Route 609)
Intersection Improvement in Fairfax County, VA
State Project No.: (NFO) 0620-029-017, P101, R201, C501
Federal Project No.: STP-5A01 (382); Contract ID No. C00103318DB71**

PROPOSAL GUARANTY

FORM C-24



COMMONWEALTH OF VIRGINIA
DEPARTMENT OF TRANSPORTATION
PROPOSAL GUARANTY

KNOW ALL MEN BY THESE PRESENTS, THAT WE Fort Myer Construction Corporation As principal, and Western Surety Company Surety, are held and firmly bound unto the Commonwealth of Virginia as obligee, in the amount of FIVE PERCENT OF THE DOLLAR VALUE OF THE BID, lawful money of the United States of America, for the payment of which, well and truly to be made, we bind ourselves, our heirs, executors, administrators, successors and assigns, jointly and severally and firmly by these presents.

SIGNED, sealed and dated this 24th Day of April, 2014

WHEREAS, the above said principal is herewith submitting its proposal for: A Design-Build Project: Braddock Road (Route 620) and Pleasant Valley Road (Route 609) Intersection Improvement

PROJECT NUMBER: Contract ID No. C00103318DB71
Federal Project No. STP-5A01 (382)
State Project No. (NFO) 0620-029-017, P101, R201, C501)

NOW, THEREFORE, the condition of the above obligee is such, that if the aforesaid principal shall be awarded the contract upon said proposal and shall within the time specified in the Specifications after the notice of such award enter into a contract and give bond for the faithful performance of the contract, then this obligation shall be null and void; otherwise to remain in full force and effect and the principal and surety will pay unto the obligee the difference in money between the amount of the bid of the said principal and the amount for which the obligee may legally contract with another party to perform the said work if the latter amount be in excess of the former; but in no event shall the liability exceed the penal sum hereof.

Fort Myer Construction Corporation

Western Surety Company

(Principal*)
By: Jose Rodriguez
(Officer, Partner or Owner) (Seal)
Jose Rodriguez, President

(Surety Company)
By: Don K. Kawamoto
Don K. Kawamoto (Attorney-in-Fact**) (Seal)

(Principal*)

333 S. Wabash Ave., Chicago, IL 60604
(Address)

By: _____
(Officer, Partner or Owner) (Seal)

By: _____
(Surety Company)

(Principal*)

(Attorney-in-Fact**) (Seal)

By: _____
(Officer, Partner or Owner) (Seal)

By: _____
(Address)

*Note: If the principal is a joint venture, each party thereof must be named and execution made by same hereon. If there is more than one surety to the bid bond, each surety must be named and execution shall be made by same hereon.

Electronic Bid Only: In lieu of completing the above section of the Contract Performance Bond, the Principal shall file an Electronic Bid Bond when bidding electronically. By signing below the Principal is ensuring the identified electronic bid bond has been executed and the Principal and Surety are firmly bound unto the Commonwealth of Virginia under the same conditions of the bid bond as shown above.

Electronic Bid Bond ID# _____ Company/Bidder Name _____ Signature and Title _____

**Attach copy of Power of Attorney

Western Surety Company

POWER OF ATTORNEY APPOINTING INDIVIDUAL ATTORNEY-IN-FACT

Know All Men By These Presents, That WESTERN SURETY COMPANY, a South Dakota corporation, is a duly organized and existing corporation having its principal office in the City of Sioux Falls, and State of South Dakota, and that it does by virtue of the signature and seal herein affixed hereby make, constitute and appoint

Joseph G Delaney, Karen M Earp, Don K Kawamoto, Individually

of Potomac, MD, its true and lawful Attorney(s)-in-Fact with full power and authority hereby conferred to sign, seal and execute for and on its behalf bonds, undertakings and other obligatory instruments of similar nature

- In Unlimited Amounts -

and to bind it thereby as fully and to the same extent as if such instruments were signed by a duly authorized officer of the corporation and all the acts of said Attorney, pursuant to the authority hereby given, are hereby ratified and confirmed.

This Power of Attorney is made and executed pursuant to and by authority of the By-Law printed on the reverse hereof, duly adopted, as indicated, by the shareholders of the corporation.

In Witness Whereof, WESTERN SURETY COMPANY has caused these presents to be signed by its Vice President and its corporate seal to be hereto affixed on this 14th day of October, 2013.



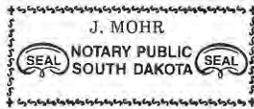
WESTERN SURETY COMPANY

Paul T. Bruflat
Paul T. Bruflat, Vice President

State of South Dakota }
County of Minnehaha } ss

On this 14th day of October, 2013, before me personally came Paul T. Bruflat, to me known, who, being by me duly sworn, did depose and say: that he resides in the City of Sioux Falls, State of South Dakota; that he is the Vice President of WESTERN SURETY COMPANY described in and which executed the above instrument; that he knows the seal of said corporation; that the seal affixed to the said instrument is such corporate seal; that it was so affixed pursuant to authority given by the Board of Directors of said corporation and that he signed his name thereto pursuant to like authority, and acknowledges same to be the act and deed of said corporation.

My commission expires
June 23, 2015



J. Mohr
J. Mohr, Notary Public

CERTIFICATE

I, L. Nelson, Assistant Secretary of WESTERN SURETY COMPANY do hereby certify that the Power of Attorney hereinabove set forth is still in force, and further certify that the By-Law of the corporation printed on the reverse hereof is still in force. In testimony whereof I have hereunto subscribed my name and affixed the seal of the said corporation this 24th day of April, 2014.



WESTERN SURETY COMPANY

L. Nelson
L. Nelson, Assistant Secretary

Authorizing By-Law

ADOPTED BY THE SHAREHOLDERS OF WESTERN SURETY COMPANY

This Power of Attorney is made and executed pursuant to and by authority of the following By-Law duly adopted by the shareholders of the Company.

Section 7. All bonds, policies, undertakings, Powers of Attorney, or other obligations of the corporation shall be executed in the corporate name of the Company by the President, Secretary, and Assistant Secretary, Treasurer, or any Vice President, or by such other officers as the Board of Directors may authorize. The President, any Vice President, Secretary, any Assistant Secretary, or the Treasurer may appoint Attorneys in Fact or agents who shall have authority to issue bonds, policies, or undertakings in the name of the Company. The corporate seal is not necessary for the validity of any bonds, policies, undertakings, Powers of Attorney or other obligations of the corporation. The signature of any such officer and the corporate seal may be printed by facsimile.



**Braddock Road (Route 620) and Pleasant Valley Road (Route 609)
Intersection Improvement in Fairfax County, VA
State Project No.: (NFO) 0620-029-017, P101, R201, C501
Federal Project No.: STP-5A01 (382); Contract ID No. C00103318DB71**

SWORN STATEMENT FORMS

C-104 & C-105



**COMMONWEALTH OF VIRGINIA
DEPARTMENT OF TRANSPORTATION**

PROJECT: 0620-029-017, P101, R201, C501

FHWA: STP-5A01 (382)

This form must be completed, signed and returned with bid; and failure to do so may result in the rejection of your bid. **THE CONTRACTOR SHALL AFFIRM THE FOLLOWING STATEMENT EITHER BY SIGNING THE AFFIDAVIT AND HAVING IT NOTARIZED OR BY SIGNING THE UNSWORN DECLARATION UNDER PENALTY OF PERJURY UNDER THE LAWS OF THE UNITED STATES.** A SEPARATE FORM MUST BE SUBMITTED BY EACH PRINCIPAL OF A JOINT VENTURE BID.

STATEMENT. In preparation and submission of this bid, I, the firm, corporation or officers, agents or employees thereof did not, either directly or indirectly, enter into any combination or arrangement with any persons, firm or corporation or enter into any agreement, participate in any collusion, or otherwise take any action in the restraint of free, competitive bidding in violation of the Sherman Act (15 U.S.C. Section 1) or Article 1.1 or Chapter 12 of Title 18.2 (Virginia Governmental Frauds Act), Sections 59.1-9.1 through 59.1-9.17 or Sections 59.1-68.6 through 59.1-68.8 of the Code of Virginia.

AFFIDAVIT

The undersigned is duly authorized by the bidder to make the foregoing statement to be filed with bids submitted on behalf of the bidder for contracts to be let by the Commonwealth Transportation Board.

Signed at Washington, DC, this 24 day of April, 20 14
County (City), STATE

Fort Myer Construction Corporation By: [Signature] Jose Rodriguez, President
(Name of Firm) (Signature) Title (print)

STATE of Washington, DC COUNTY (CITY) of Washington, DC

To-wit:

I Carolyn Burns, a Notary Public in and for the State and County(City) aforesaid, hereby certify that this day Jose Rodriguez, President

personally appeared before me and made oath that he is duly authorized to make the above statements and that such statements are true and correct.

Subscribed and sworn to before me this 24th day of April, 20 14

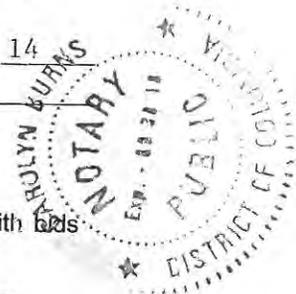
[Signature] My Commission expires 9.30.18
Notary Public Carolyn Burns

OR
UNSWORN DECLARATION

The undersigned is duly authorized by the bidder to make the foregoing statement to be filed with bids submitted on behalf of the bidder for contracts to be let by the Commonwealth Transportation Board.

Signed at _____, this _____ day of _____, 20 _____
County (City), STATE

By: _____ Title (print)
(Name of Firm) (Signature)



**COMMONWEALTH OF VIRGINIA
DEPARTMENT OF TRANSPORTATION
AFFIDAVIT**

PROJECT: 0620-029-017, P101, R201, C501

FHWA: STP-5A01 (382)

This form must be completed, signed, notarized and returned with bid; and failure to do so, may result in the rejection of your bid. A separate form must be submitted by each principal of a joint venture bid.

1. I, the firm, corporation or officers, agents or employees thereof have neither directly nor indirectly entered into any combination or arrangement with any person, firm or corporation or entered into any agreement, participated in any collusion, or otherwise taken any action in restraint of free competitive bidding in connection with such contract, the effect of which is to prevent competition or increase the cost of construction or maintenance of roads or bridges.

During the preceding twelve months, I (we) have been a member of the following Highway Contractor's Associations, as defined in Section 33.1-336 of the Code of Virginia (1970). (If none, so state).

NAME	Location of Principal Office
<u>0620-029-017, P101, R201, C501</u>	<u>STP-5A01 (382)</u>
<u>ARTBA</u>	<u>Washington, DC</u>
<u>ABC Metropolitan Washington</u>	<u>Calverton, MD</u>

2. I (we) have X , have not , participated in a previous contract or subcontract subject to the equal opportunity clause, as required by Executive Orders 10925, 11114, or 11246, and that I/We have X , have not , filed with the joint Reporting Committee, the Director of the Office of Federal Contract Compliance, a Federal Government contracting or administering agency, or the former President's Committee on Equal Employment Opportunity, all reports due under the applicable filing requirements.

Note: The above certification is required by the Equal Employment Opportunity Regulations of the Secretary of Labor [41 CFR 60-1.7(b)(1)], and must be submitted by bidders and proposed subcontractors only in connection with contracts and subcontracts which are subject to the equal opportunity clause. Contracts and subcontracts which are exempt from the equal opportunity clause are set forth in 41 CFR 60-1.5. (Generally only contract or subcontracts of \$10,000 or under are exempt.)

Currently, Standard Form 100 (EEO-1) is the only report required by the Executive Orders or their implementing regulations.

Proposed prime contractors and subcontractors who have participated in a previous contract or subcontract subject to the Executive Orders and have not filed the required reports should note that 41 CFR 60-1.7(b) (1) prevents the award of contract and subcontract unless such contractor submits a report covering the delinquent period or such other period specified by the Federal Highway Administration or by the Director, Office of Federal Contract Compliance, U.S. Department of Labor.

(Continued)

ORDER NO.:
CONTRACT ID. NO.:

Form C-105
page 2

3. The bidder certifies to the best of its knowledge and belief, that it and its principals:
- (a) Are not presently debarred, suspended, proposed for debarment, declared ineligible or voluntarily excluded from covered transactions by any Federal department or agency;
 - (b) Have not within a three year period preceding this proposal been convicted of or had a civil judgement rendered against them for commission of fraud or a criminal offence in connection with obtaining, attempting to obtain, or performing a public (Federal, State or local) transaction or contract under a public transaction; violation of Federal or State antitrust statutes or commission of embezzlement, theft, forgery, bribery, falsification or destruction of records, making false statements, or receiving stolen property;
 - (c) Are not presently indicted for or otherwise criminally or civilly charged by a governmental entity (Federal, State or local) with commission of any of the offenses enumerated above; and
 - (d) Where the bidder is unable to certify to any of the statements in this certification, the bidder shall show an explanation below.

Explanations will not necessarily result in denial of award, but will be considered in determining bidder responsibility. For any explanation noted, indicate below to whom it applies, initiating agency, and dates of action. Providing false information may result in federal criminal prosecution or administration sanctions. The bidder shall provide immediate written notice to the Department if at any time the bidder learns that its certification was erroneous when submitted or has become erroneous by reason of change circumstances.

The undersigned is duly authorized by the bidder to make the foregoing statements to be filed with bids submitted on behalf of the bidder for contracts to be let by the Commonwealth Transportation Board.

Signed at Washington, DC, this 24 day of April, 20 14
County (City), STATE

Fort Myer Construction Corp. By: [Signature] Jose Rodriguez, President
(Name of Firm) (Signature) Title (print)

STATE of Washington, DC COUNTY (CITY) of Washington, DC

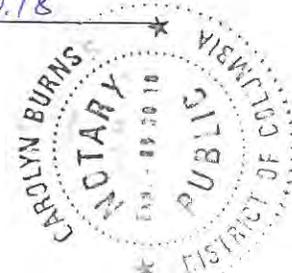
To-wit:

I Carolyn Burns, a Notary Public in and for the State and County(City) aforesaid, hereby certify that this day Jose Rodriguez, President

personally appeared before me and made oath that he is duly authorized to make the above statements and that such statements are true and correct.

Subscribed and sworn to before me this 24th day of April, 20 14

Carolyn Burns My Commission expires 9.30.18
Notary Public Carolyn Burns





**Braddock Road (Route 620) and Pleasant Valley Road (Route 609)
Intersection Improvement in Fairfax County, VA**
State Project No.: (NFO) 0620-029-017, P101, R201, C501
Federal Project No.: STP-5A01 (382); Contract ID No. C00103318DB71

CD-ROM





DESIGN-BUILD PROJECT

Price Proposal
April 24, 2014

Braddock Road (Route 620) and Pleasant Valley Road (Route 609) Intersection Improvement in Fairfax County, VA



Submitted By:



**FORT MYER
CONSTRUCTION**

In Association With:

